

Knott Mill Association

Knott Mill – Manchester

THE MASTERPLAN | JUNE 2019

10117-RP-MP01

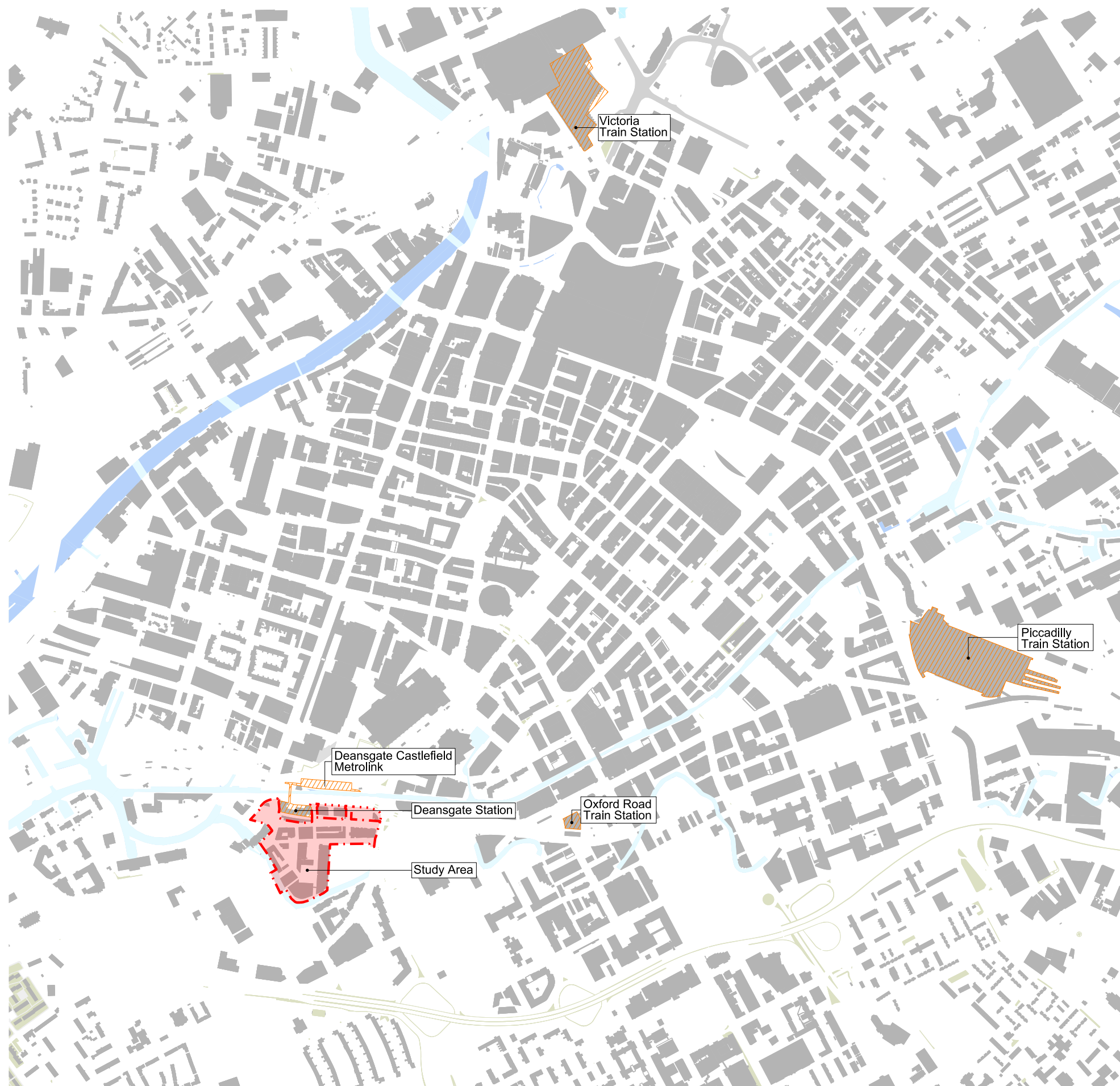


## Knott Mill

The area known as Knott Mill is located in the south west of Manchester city centre close to the location of the historic Roman fort of Mamuciam. Historically, a ford across the River Medlock also sat alongside Knott Mill and this enabled a connection between Ribchester in the north and Chester in the south. This route was an extension of the original Roman Road through Manchester which is likely to have also been the first principal route south out of the city centre

The area developed rapidly in response to its strategic location at the junction of the river and the Bridgewater Canal throughout the 19th century but fell into decline in the mid 20th century.

Since the mid 1990's this part of the city centre has undergone partial regeneration and the last two decades have witnessed a strengthening of the area's business community, particularly in the creative sector and the return of residential uses. Today, the area has become surrounded by significant development activity and this study seeks to establish a strategy to guide the future development of Knott Mill, acknowledging its unique character and improving legibility and connections with neighbouring areas.





# 1.0 EXECUTIVE SUMMARY

## 1.1 Introduction

The southern edge of the city centre will play a significant role in satisfying identified current and future demand for new dwellings and commercial space which is required to support population and economic growth. The Masterplan proposals present an opportunity to knit together a number of larger areas, such as Great Jackson Street, Whitworth Street West, Castlefield and First Street, and to plan strategically for how the unique characteristics and circumstances of this particular area can contribute to the continued growth of this part of the city centre in line with national and local planning policies.

## 1.2 The Evolving Character of Knott Mill

Knott Mill benefits from a detailed and layered historic context which has been critical in informing the development of the vision and strategies for Knott Mill’s future.

The onset of rapid industrialisation in the 19th century saw Manchester become one of the fastest growing cities in the world. The national demand for textiles, particularly cotton, began to rise resulting in a dramatic increase in mill buildings that transformed Manchester into a centre of the factory-based cotton manufacturing industry.

This process of industrial development was facilitated greatly by the introduction of canal networks, which provided the first efficient means of transporting bulk loads of goods. During the construction of the canals, a channel was cut from the River Medlock to allow water to flow through the industrial complex at Knott Mill. As part of the industrialisation process, Knott Mill also saw the introduction of rows of workers housing occupying spaces between closely packed factories, warehouses and other commercial premises.

Following the decline of industry in the mid-20th century, the area fell into a cycle of decline which saw it become under-used and partly derelict.

The 1980s and 1990s saw the partial revival of the Knott Mill area which became a hub for creative industries who were initially attracted by the low rents that an area in decline could offer. The area soon became host in part to a distinctive music and popular culture scene that developed in Manchester in the 1980s.

A masterplan exercise, undertaken in the early 1990s, resulted in the realisation of environmental improvement works and public realm works which enabled the initial regeneration of Knott Mill.

Over the last twenty five years the area has become home to a successful business community, particularly in the creative sector, alongside a developing residential community. This masterplan seeks to build on the strength of both communities and provide a variety of spaces within which both can continue to grow.

## 1.3 Study Area

Knott Mill is located in the south of Manchester city centre. The Study Area is irregular in shape and broadly bound by Hewitt Street and the Whitworth Street railway viaduct to the north, Albion Street and the Little Peter Street car park to the east, the River Medlock to the south, and the historic, southern extent of Old Deansgate to the west. The Study Area extends slightly beyond these broad boundaries at Bugle Street, Rowendale Street, and Cameron Street to the north and at two points fronting onto the Bridgewater Viaduct to the west at the interface with the Castlefield Basin.

The Study Area has an established and clear street pattern which is conducive to ease of navigation and movement by pedestrians. Whilst the urban realm is generally good, surface car parks and under utilised buildings and gap sites tend to have a negative impact on the local townscape. The quality of the public amenity space and public realm is also lacking.

Key interfaces with neighbours are currently relatively poor. The Deansgate railway viaduct and River Medlock, together with the Little Peter Street site which sits outside the Study Area (but in the neighbouring First Street Strategic Regeneration Framework), act as barriers to efficient interfaces with adjacent areas.

The Study Area is characterised by a mix of uses and is distinguished by its diversity of architectural styles and differing scales, albeit the Study Area generally can be classified as a low-to-medium rise area, particularly when compared to its emerging high-rise neighbours.

The surviving brick built warehouses are a distinctive element of the streetscape in the Study Area and reflect its previous industrial character. Many of the warehouses have been refurbished to adapt to new uses including residential apartments, offices and studios. Commercial and office uses generally dominate at the lower levels providing an element of ground floor active frontage, whilst a mix of predominantly office and residential uses generally dominate the upper levels. Small pockets of retail, leisure and cultural uses are also evident.

There has also been significant investment in larger scale, contemporary buildings on semi-derelict and brownfield land within the Study Area. Whilst these new buildings have contributed significantly to the transformation of the area, they have been brought forward in a piecemeal fashion in a way that detracts from a more cohesive, strategic development of Knott Mill which this Masterplan now aims to provide.

### Relationship with adjacent frameworks

The Study Area is surrounded by significant Development Framework Areas - Great Jackson Street to the south, First Street to the east, Whitworth Street West to the north (with the Great Northern, Manchester Central and Castlefield Quay Framework further to the north). The boundary of the Study Area has been specifically drawn to exclude developments within these Framework areas but Knott Mill has the potential to play a pivotal role in connecting all these areas to each other and with the rest of the city centre, whilst retaining its own specific character.

Development immediately adjacent to the Knott Mill study area, particularly immediately to the north of the railway viaduct along Whitworth Street is considered to be within the zone of interest of this masterplan. Development on these sites are included for context, this includes 10 – 12 Whitworth Street, which has an existing planning permission for 327 residential apartments and the land bounded by Whitworth Street West and Bugle Street which is subject to emergent residential development proposals being developed by the landowner, Network Rail in partnership with Henry Boot.

Network Rail is promoting the redevelopment of its land interests adjacent to Deansgate railway station. Proposals will provide for the retention of the existing Grade II listed building, with works to enhance the station’s entrance, internal environment, prominence and accessibility. This opportunity has huge potential to improve the profile of the station and enhance rail user experience, in addition to contributing to the regeneration of this part of the city centre.

The parts of the site that do not form part of the original station currently do not make best use of its prominent city centre location. As such, redevelopment options are being considered for the non-listed buildings, which currently include the potential for a hotel and / or residential building with complementary retail and leisure uses within the lower levels of the development.



## 1.4 Urban Design Principles

### 1.4.1 LAND USES

The Study Area's city centre location means that it is able to play a crucial role in meeting the demands arising from a growing city centre economy and population.

The ambition is for new development to create a critical mass of residential and workspace accommodation, which will in turn support the introduction of a range of amenity provision in the form of independent, small-scale retail, food and beverage outlets, which will create further activity at street level and animate this distinctive part of the City. The ambition is that the mix of commercial and residential uses will contribute to activity in the daytime, in the evening and at weekends. It is not anticipated that the area is necessarily suitable for night time uses such as bars, and those types of uses will only be considered if appropriate and properly managed.

The type of space provided and the mix should be carefully considered with a view to providing commercial spaces into which the area's existing businesses can expand (rather than having to leave Knott Mill and look for space elsewhere) and homes which will support the area's existing residential population as those residents mature and look for different types of accommodation.

### 1.4.2 CREATING A SENSE OF PLACE

Key buildings of architectural, historical and cultural value together with the historic urban grain are seen as an intrinsic part of the character of Knott Mill and are central to the Masterplan strategy. Such features contribute to creating a sense of place and are fundamental to the area's local distinctiveness.

### 1.4.3 STREET HIERARCHY

There is a clear opportunity to enhance the existing pedestrian connections and create new connections through the Study Area which will further increase activity in the area. The Masterplan strategy also aims to bridge the separations which exist between adjacent development areas and the City centre. It is intended the existing street hierarchy with regards to vehicular traffic remains largely unchanged except for the introduction of traffic calming measures to discourage 'rat running' and create a more welcoming environment for pedestrians.

### 1.4.4 PUBLIC REALM AND AMENITY

Whilst the quality of Knott Mill's urban realm is reasonable, its public amenity space is lacking. At the heart of the Masterplan is the creation of a new public space, a "pocket park" at the junction of Commercial Street and Constance Street surrounded by 'independent' operators offering 'café' and Food and Beverage opportunities, and providing a setting for existing and new buildings.

A further, key part of the place-making strategy is to ensure Knott Mill is well connected to neighbouring large public spaces at First Street and at

the Deansgate Square development. These will provide community focus and amenity space for existing and future residents, businesses and visitors to Knott Mill.

The Study Area's close proximity to all available modes of public transport means other public spaces further afield are also easily accessible.

### 1.4.5 CONNECTIVITY AND PEDESTRIAN ENVIRONMENT

As the Deansgate railway viaduct and the River Medlock act as physical and psychological barriers to pedestrian movement, the Masterplan places significant importance on the creation and enhancement of walking routes to integrate Knott Mill with the wider city centre.

A significant linear pedestrian route runs south-west to north-east as part of the Great Jackson Street SRF and this route terminates in the large public plaza within the Deansgate Square development. This axial route could be extended via a pedestrian bridge link to Commercial Street, and the new "pocket park", via the 4 Commercial Street and Commercial Wharf plots.

In addition, the Masterplan proposals promote the extension of the limited existing riverside walkway provided by the Hills Quay development. This river walk could extend across the Little Peter St car park site (within the First Street SRF) to enhance pedestrian connections between Knott Mill and First Street and also take advantage of the currently undervalued amenity asset of the River Medlock.

The principal interface via the historic southern extent of Old Deansgate will increase in prominence and see significantly increased pedestrian flows in the direction to and from the city centre via Deansgate. This area should be transformed into an environment which prioritises pedestrians over vehicular traffic. A shared paving scheme is envisaged, reducing the definition between road and pavement edges, reducing the width of the carriageway and introducing planting, trees, street furniture together with improved street lighting. These changes will create a gateway that is in keeping with the vibrant and animated city centre neighbourhood that this Masterplan promotes.

### 1.4.6 SUSTAINABLE AND SAFE DEVELOPMENT

Knott Mill must continue to develop as a community that is able to thrive in Manchester's low carbon future.

All development in Knott Mill should be carefully considered in order to help to facilitate the changes in energy efficiency, energy generation and energy usage needed to meet the target set in the draft Manchester Zero Carbon Framework, for the city to be zero carbon by 2038.

### 1.4.7 HEIGHT AND DENSITY

The Masterplan proposals have been developed in response to detailed contextual appraisals, including economic development analysis to inform use and quantum. The vision aims to mediate the scale of development between the city centre and emerging development areas at the southern edge of the city centre. A limit on scale has been set to ensure development height is capped well below the scale of the Great Jackson Street and Whitworth Street West developments. Development in Knott Mill should reinforce the area's own distinctive character, rather than seeking to emulate adjacent developments in nature and scale.

### 1.4.8 DEVELOPER CONTRIBUTIONS

It is envisaged Manchester City Council will utilise all reasonable resources and mechanisms available to secure appropriate financial contributions from developers to allow the proposed public realm and infrastructure improvements to come forward alongside the delivery of development sites within the Study Area.

## 1.5 Illustrative Masterplan

An illustrative masterplan has been developed which is intended to identify opportunities. The intention is not to prescribe a form and scale of development but rather provide an overall strategy against which individual, future planning applications will be tested.

Within the Study Area, three distinct zones have been identified:

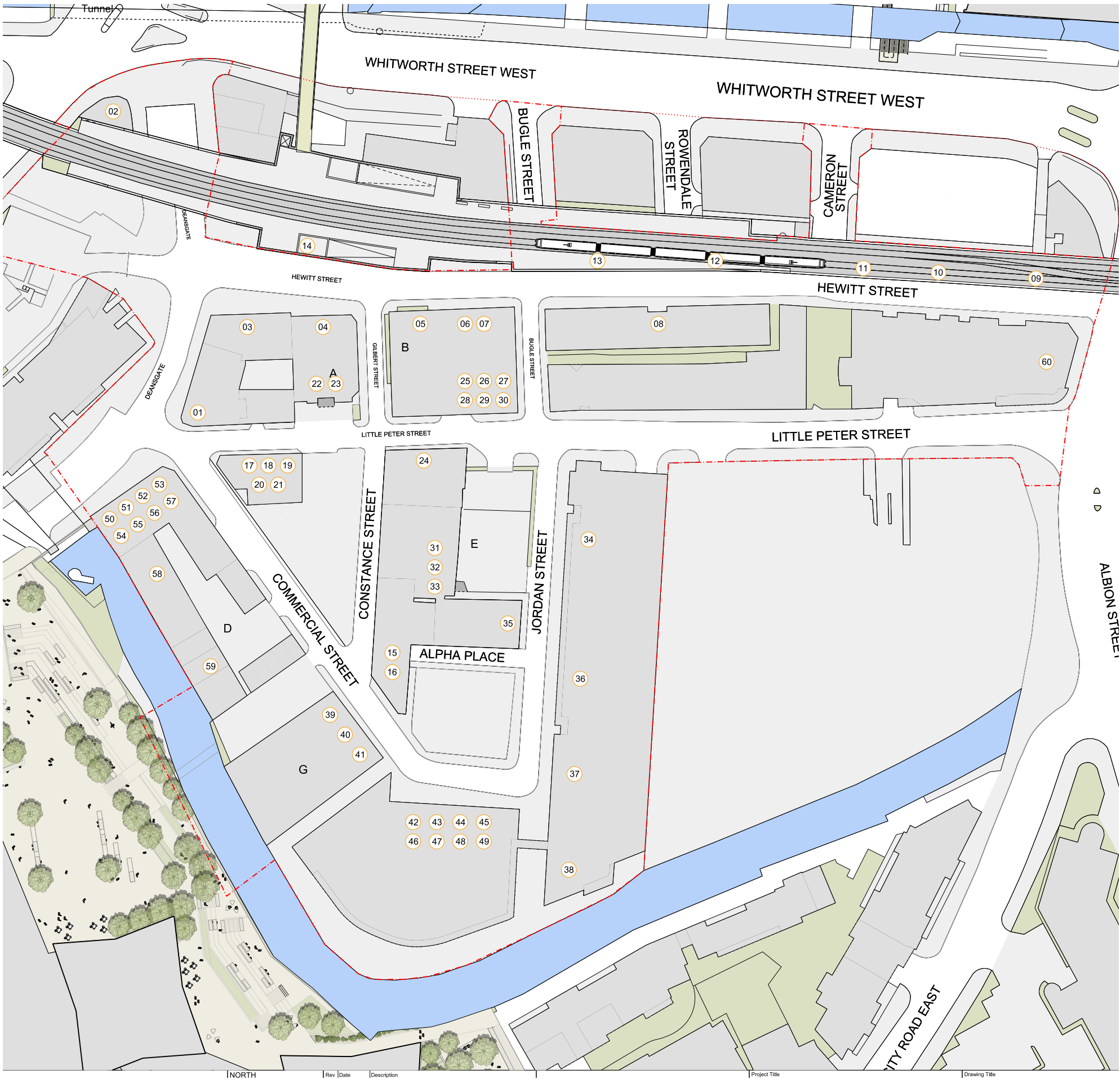
- The Band - The strip of midrise development along northern edge of the study area. A mix of recently developed buildings either side of the character assets of 8 Hewitt Street and the former St. Peter's School.
- The Core - The central lower rise heart of Knott Mill which comprises the remnants of the historic buildings of most character value.
- The Fringe - The larger plots and large scale developments along the river edge capable of mediating in scale with the Great Jackson Street developments

1.0 EXECUTIVE SUMMARY

1.6 Existing Workforce

Key:

Key Ref.	Business Name:	Business Description:
	Deansgate	
01	Haughton Warburton Ltd.	Surveyor
02	Atlas Bar	Bar
	Hewitt Street	
03	Castlefield Gallery	Art Gallery
04	Canning O'Neill Ltd.	Estate Agents
05	Forever Manchester	Charity
06	Mobedia	Digital Marketing Agency
07	Spoken Image	Media Company
08	Manchester School of Acting	Higher Education
09	D P Stanley	Garage Services
10	Mancoco	Coffee Shop
11	Mail Box Express	Courier Service
12	Jon Male Ltd.	Furniture Retailer
13	G T Autos	Garage Services
14	Prime	Recruitment Agency
	Constance Street	
15	Lewis & Hickey Ltd.	Architects
16	Tim Groom Architects	Architects
	Little Peter Street	
17	Capital and Centric	Developers
18	Harrison Ince	Architects
19	Sherpa Recruitment	Recruitment Agency
20	Studio eNaR	Graphic Designer
21	Voicescape	Software Developer
22	Lion Eyes Television Ltd.	TV & Film Production
23	The Den Post Production	Post Production Company
24	Ralph Capper Interiors Ltd.	Furniture Retailer
25	AABC Register (Register of Architects Accredited in Building Conservation)	Accreditation Body
26	Artorius Wealth	Asset Allocation Office
27	Hepworth Acoustics	Acoustic Consultant
28	Lloyd Evans Prichard Ltd.	Architects
29	MC2 Ltd.	Marketing Agency
30	Jonathan Davidson Architects	Architects
	Jordan Street	
31	Christian Douglass LLP	Accountant
32	Omega Fire Engineering Ltd.	Fire Engineers
33	Weber Shandwick	Public Relations
34	Just H Architects	Architects
35	Zerum	Property Consultancy
36	Fletcher-Rae Ltd.	Architects
37	Cast UK Ltd.	Recruitment Agency
38	Croft Transport Solutions	Transport Consultancy
	Commercial Street	
39	Cheetham Bell	Advertising Agency
40	ROC Consulting	Civil Engineers
41	Stride Treglown	Architects
42	Better Placed Recruitment Ltd.	Recruitment Agency
43	Gibson Hollyhomes Ltd.	Recruitment Agency
44	Potter Rees Dolan	Solicitors
45	Burgess Bowen Financial Services Ltd.	Financial Advisor
46	Fibre Safe Ltd.	Asbestos Testing Service
47	Keppie Massie	Surveyors and Property Consultants
48	Pest Control Manchester	Pest Control
49	Premier Residential Lettings	Estate Agents
50	Enigma Lighting	Lighting Consultant
51	E X P Live Ltd.	Marketing and Advertising Agency
52	Hulley & Kirkwood	Engineers
53	Incorporate Travel Management	Travel Agency
54	Spacelvader Design	Interior Design
55	Tetra Marketing	Marketing Agency
56	The Chase Creative Consultants Ltd.	Advertising, Graphic Design & Film Production
57	GIA	Surveyors
58	SimpsonHaugh and Partners	Architects
59	Stephenson Studio Ltd.	Architects
	Albion Street	
60	Manchester Smiles	Dental Practice



1.0 EXECUTIVE SUMMARY

Key Ref.	Business Name:	Business Description:	Address:	Postcode:	Estimated Employees:
	Deansgate				
01	Houghton Warburton Ltd.	Surveyor	361 Deansgate, Manchester	M3 4LG	5
02	Atlas Bar	Bar	376 Deansgate, Manchester	M3 4LY	5
	Hewitt Street				
03	Castlefield Gallery	Art Gallery	2 Hewitt Street, Manchester	M15 4GB	6
04	Canning O'Neill Ltd.	Estate Agents	6 Hewitt Street, Manchester	M15 4GB	15
05	Forever Manchester	Charity	8 Hewitt Street, Manchester	M15 4GB	15
06	Mobedia	Digital Marketing Agency	8 Hewitt Street, Manchester	M15 4GB	10
07	Spoken Image	Media Company	8 Hewitt Street, Manchester	M15 4GB	5
08	Manchester School of Acting	Higher Education	14-32 Hewitt Street, Manchester	M15 4GB	5
09	D P Stanley	Garage Services	Arch 82 Hewitt Street, Manchester	M15 4GB	5
10	Mancoco	Coffee Shop	Arch 84 Hewitt Street, Manchester	M15 4GB	2
11	Mail Box Express	Courier Service	Arch 85 Hewitt Street, Manchester	M15 4GB	7
12	Jon Male Ltd.	Furniture Retailer	Arch 87 Hewitt Street, Manchester	M15 4GB	1
13	G T Autos	Garage Services	Arch 88 Hewitt Street, Manchester	M15 4GB	5
14	Prime	Recruitment Agency	Arch 91 Hewitt Street, Manchester	M15 4GB	8
	Constance Street				
15	Lewis & Hickey Ltd.	Architects	7 Constance Street, Manchester	M15 4JQ	8
16	Tim Groom Architects	Architects	7 Constance Street, Manchester	M15 4JQ	20
	Little Peter Street				
17	Capital and Centric	Developers	2-4 Little Peter Street, Manchester	M15 4PS	7
18	Harrison Ince	Architects	2-4 Little Peter Street, Manchester	M15 4PS	10
19	Sherpa Recruitment	Recruitment Agency	2-4 Little Peter Street, Manchester	M15 4PS	8
20	Studio eNaR	Graphic Designer	2-4 Little Peter Street, Manchester	M15 4PS	6
21	Voicescape	Software Developer	2-4 Little Peter Street, Manchester	M15 4PS	5
22	Lion Eyes Television Ltd.	TV & Film Production	3-11 Little Peter Street, Manchester	M15 4PS	5
23	The Den Post Production	Post Production Company	3-11 Little Peter Street, Manchester	M15 4PS	5
24	Ralph Capper Interiors Ltd.	Furniture Retailer	10A Little Peter Street, Manchester	M15 4PS	4
25	AABC Register (Register of Architects Accredited in Building Conservation)	Accreditation Body	21 Little Peter Street, Manchester	M15 4PS	5
26	Artorius Wealth	Asset Allocation Office	21 Little Peter Street, Manchester	M15 4PS	10
27	Hepworth Acoustics	Acoustic Consultant	21 Little Peter Street, Manchester	M15 4PS	10
28	Lloyd Evans Prichard Ltd.	Architects	21 Little Peter Street, Manchester	M15 4PS	12
29	MC2 Ltd.	Marketing Agency	21 Little Peter Street, Manchester	M15 4PS	15
30	Jonathan Davidson Architects	Architects	21 Little Peter Street, Manchester	M15 4PS	2
	Jordan Street				
31	Christian Douglass LLP	Accountant	2 Jordan Street, Manchester	M15 4PY	11
32	Omega Fire Engineering Ltd.	Fire Engineers	2 Jordan Street, Manchester	M15 4PY	20
33	Weber Shandwick	Public Relations	2 Jordan Street, Manchester	M15 4PY	20
34	Just H Architects	Architects	3 Jordan Street, Manchester	M15 4PY	10
35	Zerum	Property Consultancy	4 Jordan Street, Manchester	M15 4PY	17
36	Fletcher-Rae Ltd.	Architects	5 Jordan Street, Manchester	M15 4PY	22
37	Cast UK Ltd.	Recruitment Agency	7 Jordan Street, Manchester	M15 4PY	14
38	Croft Transport Solutions	Transport Consultancy	9 Jordan Street, Manchester	M15 4PY	7
	Commercial Street				
39	Cheetham Bell	Advertising Agency	6 Commercial Street, Manchester	M15 4PZ	20
40	ROC Consulting	Civil Engineers	6 Commercial Street, Manchester	M15 4PZ	12
41	Stride Treglown	Architects	6 Commercial Street, Manchester	M15 4PZ	15
42	Better Placed Recruitment Ltd.	Recruitment Agency	10 Commercial Street, Manchester	M15 4PZ	10
43	Gibson Hollyhomes Ltd.	Recruitment Agency	10 Commercial Street, Manchester	M15 4PZ	12
44	Potter Rees Dolan	Solicitors	12 Commercial Street, Manchester	M15 4PZ	14
45	Burgess Bowen Financial Services Ltd.	Financial Advisor	14 Commercial Street, Manchester	M15 4PZ	15
46	Fibre Safe Ltd.	Asbestos Testing Service	14 Commercial Street, Manchester	M15 4PZ	5
47	Keppie Massie	Surveyors and Property Consultants	14 Commercial Street, Manchester	M15 4PZ	5
48	Pest Control Manchester	Pest Control	14 Commercial Street, Manchester	M15 4PZ	5
49	Premier Residential Lettings	Estate Agents	16 Commercial Street, Manchester	M15 4PZ	7
50	Enigma Lighting	Lighting Consultant	2 Commercial Street, Manchester	M15 4RQ	7
51	E X P Live Ltd.	Marketing and Advertising Agency	2 Commercial Street, Manchester	M15 4RQ	5
52	Hulley & Kirkwood	Engineers	2 Commercial Street, Manchester	M15 4RQ	15
53	Incorporate Travel Management	Travel Agency	2 Commercial Street, Manchester	M15 4RQ	10
54	SpacInvader Design	Interior Design	2 Commercial Street, Manchester	M15 4RQ	10
55	Tetra Marketing	Marketing Agency	2 Commercial Street, Manchester	M15 4RQ	5
56	The Chase Creative Consultants Ltd.	Advertising, Graphic Design & Film Production	2 Commercial Street, Manchester	M15 4RQ	10
57	GIA	Surveyors	2 Commercial Street, Manchester	M15 4RQ	6
58	SimpsonHaugh and Partners	Architects	Riverside Mews, 4 Commercial Street, Manchester	M15 4RQ	70
59	Stephenson Studio Ltd.	Architects	3 Riverside Mews, 4 Commercial Street, Manchester	M15 4RQ	20
	Albion Street				
60	Manchester Smiles	Dental Practice	16 Albion Street, Manchester	M15 4GB	4
EXISTING ESTIMATED TOTAL OF EMPLOYMENT:					629

The plan/schedule adjacent identifies the number employed and types of employment that are located in Knott Mill\* and illustrates how successful the area has been in providing space which has helped to support the development of a community of businesses, particularly in the creative industries. The aim of the masterplan is to provide more and better space, alongside the existing workspace, which helps those businesses to grow and encourages them to stay in this thriving part of the city centre.

The Masterplan will modernise and strengthen a district which provides less expensive office and retail/leisure space for the creative industries, small technical services companies and independent traders, groups which are in danger of being crowded out of the city centre to make way for new corporate development and investment.

\*note employment numbers based on survey carried out in November 2017





## 1.7 Economic and Market Context

The Knott Mill Masterplan will contribute to the economic and housing ambitions of the City Council and Greater Manchester Combined Authority, as well as the wider aims sets out in 'Our Manchester'.

This document describes a flexible masterplan. New development will be commercially led, with at least 60% of the new space created being devoted to employment generating uses. Indicative drawings have been prepared to show that it would be possible to create a development of up to 230,000 sqft (gross) of new commercial, retail and leisure space in addition to up to 150 new homes within Knott Mill.

The ground and first floors of all new development and refurbishment of existing property should, where practicable, incorporate workspace designed to accommodate creative industries and independent operators. In addition to this there is potential for further workspace to be developed on the upper floors of new developments. The new workspace created could support as many as 750 new full time jobs (920 total if part time employment is included) and would provide a broad range of opportunities, including a considerable number of graduate level positions in technical occupations, and a significant number of entry level jobs as well as part time opportunities, contributing to the local employment and the inclusive growth agenda of the City Council. This new employment, along with the current workforce, will generate significant taxation income over the next ten years for the Treasury and City Council.

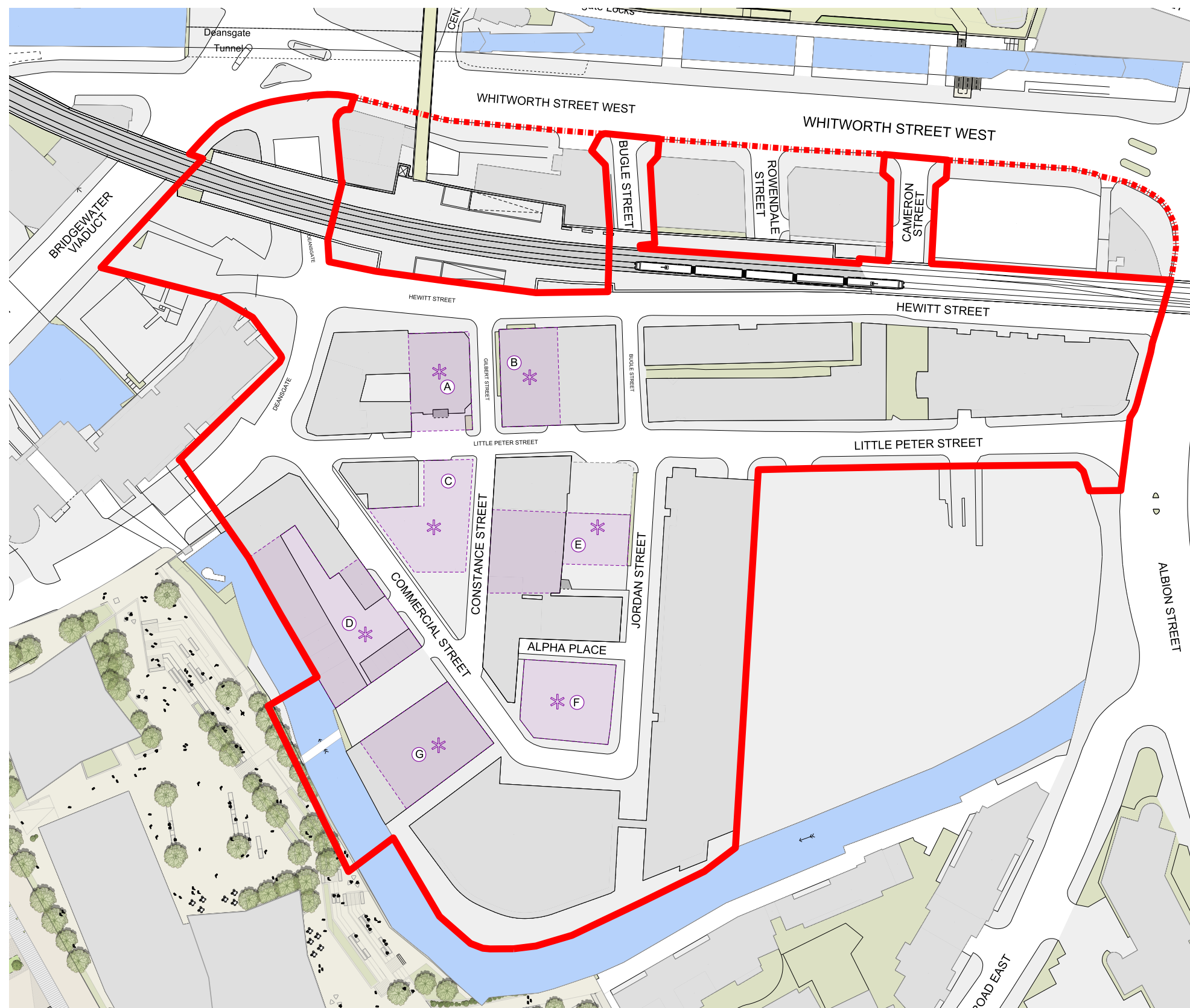
Alongside this, a new residential development of around 150 homes could be created. This would help accommodate the skilled workforce needed to support economic growth. A mix of dwellings should be provided including accommodation suitable for families.

The combination of new housing and commercial development will renew and modernise a distinctive and authentic mixed use district at the heart of the city centre.

Retaining and growing creative businesses within Knott Mill will support the ambitions of the Greater Manchester Industrial Strategy - which identifies the digital, creative and media industries as one of the four key drivers of future productivity which will support Greater Manchester's ambitions of becoming a leading European digital city region.

### 1.7.1 WORKSPACE SUPPORT

Rental levels are to be set to encourage occupation and support of new business. All efforts are to be made to retain, or relocate within the area, existing businesses that may be affected by any redevelopment proposals.



Masterplan Proposals

Masterplan Boundary



Plots outside the Masterplan boundary which are within the zone of interest of the Masterplan and which need to interact with it







## 2.0 INTRODUCTION

### 2.1 Purpose of the Report

This Masterplan has been prepared to guide the future development of Knott Mill, ensuring that opportunities for commercially-focused development are maximised whilst simultaneously embracing the unique character of Knott Mill, given the area’s cultural significance to Manchester. The blending of quality development and urban design, while being cognisant of Knott Mill’s particular identity and grain, will result in a distinctive, attractive and successful residential-led, creative industries/mixed-use neighbourhood that sits well with and complements its neighbours.

### 2.2 Key Characteristics and Opportunities

#### 2.2.1 OVERVIEW

It is essential that city centre neighbourhoods are desirable places to live and work. Knott Mill therefore has an important role to play in fuelling the economic growth of the city by responding to the need to attract and retain talent. The southern extent of the city centre has the potential to be a focus of population growth given the recent and future development underway as part of the Great Jackson Street and First Street Frameworks.

The Study Area is well-placed to become a key contributor to the city’s quality of life offer through the growth of residential and commercial development which also embraces the established predominant use of the area by the creative industry sector.

#### 2.2.2 CITY CENTRE LOCATION

The Study Area is located between Castlefield, Whitworth Street West and First Street and is close to an array of employment, retail and leisure provisions, as well as residential dwellings, all of which contribute to Manchester’s reputation as a vibrant city. Furthermore, a range of sporting and entertainment venues, galleries, theatres, restaurants, bars, and cafes attract visitors, students, residents and workers alike to the area.

In the immediate vicinity, is the city centre’s expanding cultural offer at First Street’s HOME development - a contemporary art, theatre and film centre. In addition, the delivery of Factory at St John’s - a unique arts space, will further bolster this part of the city’s cultural offer.

The Study Area continues to attract those who seek affordable workspace and want to live and/or work within the city centre. There is an opportunity to further capitalise on the Study Area’s locational attributes and distinctive character to accommodate those who wish to live in, work in and enjoy Knott Mill and its surrounding areas.

#### 2.2.3 KEY DEVELOPMENTS ADJACENT

Increasing regeneration activity is underway in all directions immediately adjacent to the Study Area. First Street is developing into a cultural and commercial destination with growing employment opportunities. The First Street Development Framework area includes the Little Peter Street car park site which will influence the setting of Knott Mill as development proposals come forward.

Manchester Central and Whitworth Street West are located immediately to the north of the Study Area and large-scale, mixed-use developments are proposed in both of these areas. This accelerated transformation of the southern edge of the city centre builds on significant regeneration activity that has already taken place including the provision of high quality public realm at the Deansgate-Castlefield Metrolink stop and First Street.

To the south of the Study Area lies the Great Jackson Street area, which includes the Deansgate Square development. This area is currently experiencing substantial regeneration activity and investment and will deliver a significant number of new homes, as well as a step change in the scale of buildings in relation to Knott Mill. However, this area is presently severed from the city centre by the Study Area. Knott Mill could therefore take advantage of an increase in footfall as a result of these developments. This increased footfall, in turn, has the potential to contribute to the vibrancy and around the clock activity at Knott Mill.

The connection between Knott Mill, the rest of the city centre to the north and the neighbourhoods to the south is critical to the success of the regeneration of the southern-edge of the city centre. Indeed, as a result of the ongoing regeneration, the southern extent of the city centre is shifting southwards (as outlined in the City Centre Strategic Plan), reorienting the Study Area from being a city-fringe location to being a more city centre location. The Study Area will also form a pivotal link between several key neighbourhoods in the southern part of the city centre.

#### 2.2.4 TRANSPORT CONNECTIONS

The Study Area is served extensively by excellent transport links and such areas are becoming increasingly attractive destinations due to their ease of access to employment opportunities.

The Study Area is adjacent to Deansgate-Castlefield Metrolink tram stop and Deansgate Rail Station which provides a direct rail link between Piccadilly, Victoria and Oxford Road Rail Stations and onto Manchester Airport and wider national rail destinations.

There are also bus services, including a Metroshuttle service immediately to the north of the Study Area at Whitworth Street West. Other bus services run from stops at Deansgate/Chester Road (A56) and Albion Street/Medlock Street - both immediately adjacent to the Study Area.

In addition, there is excellent access to the city’s inner ring road (Mancunian Way) and arterial routes heading out to the M60 and wider road network.

#### 2.2.5 NATURAL ENVIRONMENT

The River Medlock forms the southern boundary of the Study Area. It flows past a Grade II listed sluice gate to the south west of the Study Area and into a culvert which passes beneath the Castlefield Basin before discharging into the River Irwell.

Green spaces are a short walk away including St John’s Gardens and Hulme Park. Further large areas of public space are available at Castlefield, First Street and will be available soon at the Deansgate Square public plaza.

The regeneration of Knott Mill will enhance the interface between the Study Area and the River Medlock unlocking this hidden asset and proposing new planting along key pedestrian routes.

2.2.6 AVAILABLE LAND

The Study Area contains several vacant and under utilised buildings and plots, alongside an abundance of previously developed land parcels. Land ownership is fractured and there are several small potential development plots which could be combined without impacting upon the established historic street pattern.

2.2.7 HISTORIC STREET PATTERN AND CHARACTER ASSETS

Despite the absence of listed buildings within the Study Area, there are several buildings with heritage, architectural, and cultural value which contribute positively to the character of Knott Mill.

These valued buildings are central to the Masterplan strategy and will reinforce the area’s character and enhance the sense of place. All buildings identified as character assets and of cultural merit will be retained.

The historic street pattern will remain unaltered. The street hierarchy and landscape proposals will seek to enhance connections to adjacent neighbourhoods and development areas.

2.2.8 KNOTT MILL AREA DEVELOPMENT STRATEGY 1992

A masterplan exercise, undertaken in the early 1990s, resulted in the realisation of environmental improvement and public realm works which kick started the initial regeneration of Knott Mill. This new masterplan builds upon some of the principles set out in the 1992 development strategy, which as yet, remain unrealised.

2.2.9 PLANNING POLICY ENVIRONMENT

The south-western part of the city centre will play a significant role in satisfying identified current and future demand for new dwellings and commercial space in new neighbourhoods, which is required to support population and economic growth.

Knott Mill will contribute significantly to connecting the Great Jackson Street development area, the Castlefield Conservation area, the First Street development area, and the employment, leisure and retail opportunities available in the city centre.

2.3 Study Area Vision

Given Knott Mill’s key characteristics and opportunities, it is clear that there is significant potential for the area to become a more distinctive and successful mixed-use neighbourhood of choice.

This Masterplan provides a detailed contextual site analysis which has informed a strategic masterplanning process. This has identified a set of development principles which provides the framework to guide all new development proposed within the Study Area.

A series of development and urban design principles are set out in Section 5 of this Masterplan, accompanied by an Illustrative Masterplan and supporting diagrams which demonstrate how this vision can be realised. The strategies identify an appropriate mix of uses, appropriate building heights and density, suitable materiality and a place-making strategy focused on creation and enhancement of connections alongside the promotion of active frontages.

2.4 Report Structure

This document is structured as follows;

**Section 1.0 Executive Summary:** provides a concise overview of the economic context, the Study Area extents and urban design principles.

**Section 2.0 Introduction:** provides an introductory overview of the Study Area’s key characteristics and opportunities, as well as a vision for the Study Area.

**Section 3.0 Strategic Planning and Regeneration Context:** describes the relevant strategic planning policy and regeneration context.

**Section 4.0 Study Area Appraisal:** provides a detailed site analysis and considers Knott Mill’s unique opportunities, constraints and key issues.

**Section 5.0 Framework Development and Urban Design Principles:** identifies a range of development and urban design principles which will be used to guide all future development within the Study Area.

**Section 6.0 Economic and Social Value:** describes how the outcomes of the Knott Mill Masterplan will contribute to the economic and housing ambitions of the City Council and Greater Manchester Combined Authority.

2.5 Planning Status

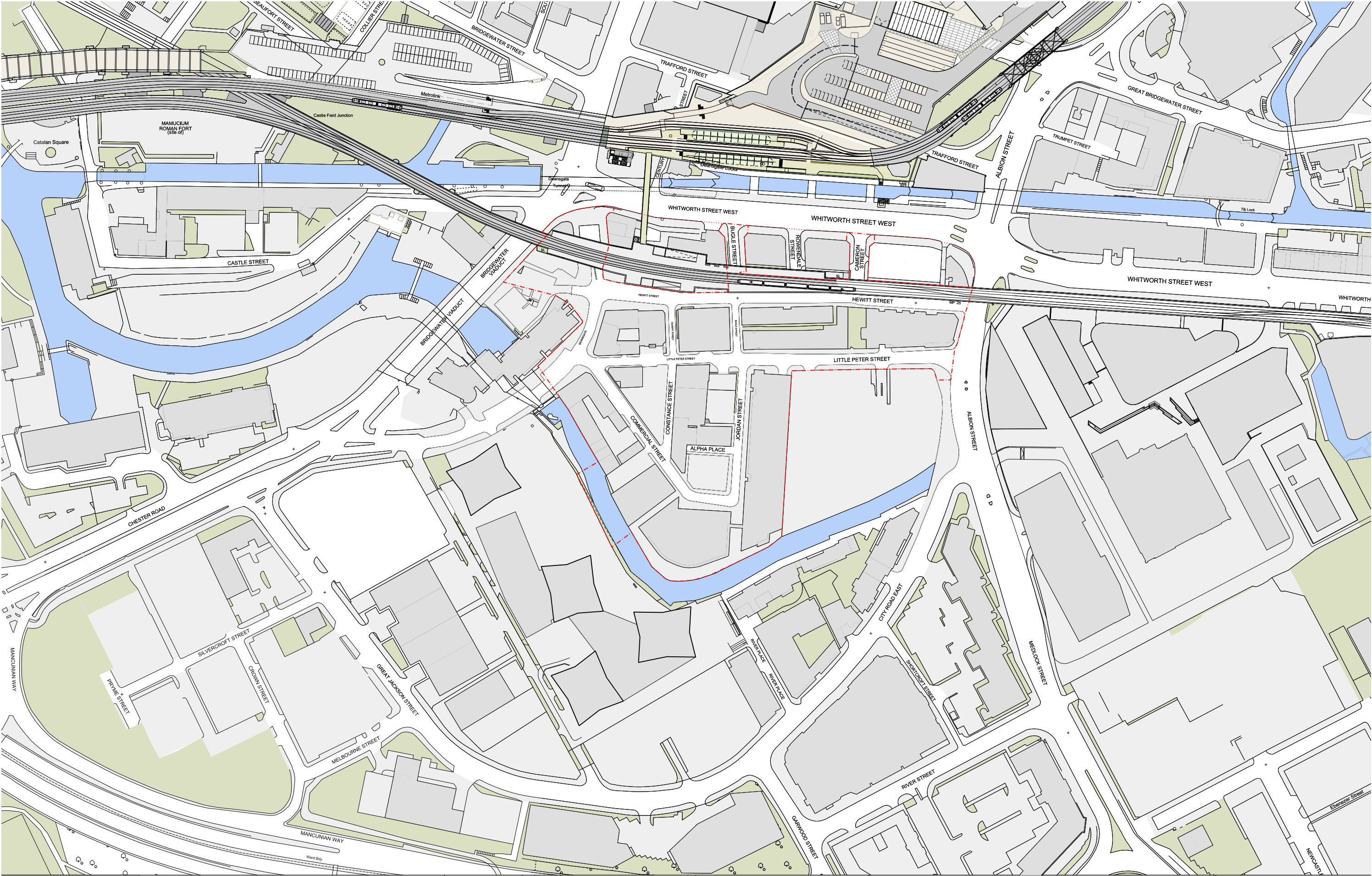
Following the endorsement of this Masterplan, this document will become a material planning consideration in the determination of all planning applications relative to the Study Area. Whilst it does not form part of the Development Plan, it has been prepared to be consistent with the adopted Manchester City Council Core Strategy and National Planning Policy Framework.







3.0 STRATEGIC PLANNING AND REGENERATION CONTEXT





### 3.1 Introduction

#### 3.1.1 OVERVIEW

Knott Mill represents a strategic opportunity to support and deliver a number of important policy objectives at the City and Greater Manchester level. This section of the report provides a summary of key policies that have influenced the preparation of this document and the content of the proposals.

### 3.2 Strategic Planning Policy Context

#### 3.2.1 HOUSING WHITE PAPER

On 30th January 2017, the Government published the White Paper “Fixing our broken housing market”. The paper intends to boost housing supply and create a more efficient housing market by taking a comprehensive approach to tackle failures at every point in the system. The paper sets out that there is a need for more land for homes, to ensure homes are built quickly, in order to diversify the housing market and to take steps to improve the current situation.

In order to meet one of the key aims of ensuring that there is enough land for homes, the paper makes a commitment to use land more efficiently which is particularly relevant for development in city centres. The proposals include maximising contributions from brownfield land and goes on to state that “the presumption should be that brownfield land is suitable for housing unless there are clear and specific reasons to the contrary”.

Further, the White Paper states that better use should be made of locations where demand is high and available land is limited. In this sense, the White Paper seeks to promote higher density housing, stating that “authorities and applicants need to be ambitious about what sites can offer, especially in areas where demand is high and land is scarce, and where there are opportunities to make effective use of brownfield land given the strong economic and environmental benefit”.

#### 3.2.2 NORTHERN POWERHOUSE

There is a growing recognition, at both a local and national level, that by joining together the cities of the North of England, an increasingly powerful economic area can be created that can help to re-balance and grow the national economy, providing the ability for them to compete internationally. Manchester is at the forefront of this movement, and the city centre, as a key driver of the economies of both Greater Manchester and the North West, will be a crucial component in this connected Northern Powerhouse.

In the 2014 Autumn Statement, George Osborne introduced the concept of the Northern Powerhouse. This is a vision for the North of England, based on the idea of combining major strategic investment into transport, science

and innovation and creating creative clusters, and devolution. The Statement pledged £7 billion to support the northern economies to develop and deliver higher levels of productivity, thereby enabling these economies to be a more effective counterbalance to the London economy.

The announcement also led to the appointment of Greater Manchester’s directly elected Mayor; with responsibility for local transport budgets, a £300m housing investment fund and powers relating to housing and planning.

A subsequent budget in July 2015 reconfirmed the Government’s commitment to the Northern Powerhouse by announcing further devolution deals with the Sheffield City Region and Leeds, West Yorkshire and partner authorities.

To put this into perspective, the key City Regions of the North of England taken together represent the 10th largest EU economy (£289 billion), offering 19% UK GDP and with tremendous growth potential estimated at £37bn to 2020.

To capitalise on this growth potential, Manchester must positively forward plan and grasp opportunities to create certainty for key development opportunities such as those which exist in Knott Mill.

#### 3.2.3 DRAFT GMSF

The Greater Manchester Combined Authority published the draft Greater Manchester Spatial Framework (GMSF) on 20th October 2016.

The aim of the draft overarching spatial plan is to provide development opportunities across the whole of Greater Manchester to ensure that investment and growth in houses and jobs happens but also benefits residents and makes Greater Manchester a better place to live and work.

In total, the Draft Plan proposes the following development requirements over the Plan period (2015-2035):

- 227,200 homes
- 4,000,000 sqm of industrial and warehousing floor space; and
- 2,450,000 sqm of office floor space.

The Plan identifies a number of Strategic Locations which will make significant contributions to development requirements set out above. The city centre is one of the ten identified Strategic Locations and will be specifically focused on as being the primary business location in the country outside London, as well as being required to deliver housing. Over the period 2015 – 2035, the policy sets a target for the city centre of 1,250,000 sqm of new office floor space and around 40,000 new additional dwellings.

Knott Mill also has the potential to contribute strongly to a wide range of strategic policy objectives within the draft GMSF as summarised below:

**Policy SL1 City Centre** states that this area will provide the primary focus for business, as well as diversifying the city centre’s offer with retail, leisure, culture and tourist uses to complement the economic role. Policy SL1 also acknowledges the increasingly important role of residential accommodation in the city centre, and its capability to enhance the mix and quality of accommodation available. The continued growth of the city centre requires

all the priorities to be balanced; namely protecting its fundamentally important economic role whilst also accounting for a very large increase in the residential population, as well as taking advantage of the lifestyle opportunities that the city centre location can offer.

**Policy GM1 Delivering a Successful Greater Manchester** seeks to focus development as much as possible towards the existing urban areas, and particularly previously-developed land within urban areas to assist the continued regeneration. Moreover there will be a presumption towards high density development for dwellings and employment space in highly accessible locations and near to public transport stops which are served frequently.

**Policy GM3 Offices** states that new office floor space will be focused in a small number of key locations, including the city centre of Manchester and Salford, which will help to further enhance its position as the premier office location outside London.

**Policy GM4 Retail, Leisure and Tourism** also identifies the city centre as a key location for enhancing the retail, tourism and leisure opportunities which are vital to providing the quality of life that will help to attract people to live and work in and enjoy Greater Manchester.

**Policy GM19 Design** places great emphasis on high quality design being fundamental to creating high quality, attractive places for the future success of Greater Manchester. It is essential that all new developments help to enhance local distinctiveness and identity, and therefore will be required to positively respond to their surroundings, the quality of the area, the historic context and the landscape character.

### 3.0 STRATEGIC PLANNING AND REGENERATION CONTEXT

## 3.3 Local Planning Context

#### 3.3.1 OUR MANCHESTER STRATEGY 2016 – 2025

The Our Manchester Strategy sets out a vision of a new model for sustainable economic growth based around a more connected and greener city region, where all residents are able to contribute to, and benefit from, sustained prosperity and a good quality of life.

It seeks to secure Manchester’s position as a world class city:

- With a competitive, dynamic and sustainable economy that draws on distinctive strengths in science, advanced manufacturing, culture, creative and digital business, cultivating and encouraging new ideas;
- With highly skilled, enterprising and industrious people;
- That is connected internationally and within the UK;
- That plays a full part in limiting the impacts of climate change;
- Where residents from all backgrounds feel safe, can aspire, succeed and live well; and,
- That is clean, attractive, culturally-rich, outward-looking and welcoming.

The Strategy is then structured under five key themes, namely a thriving and sustainable city, a highly skilled city, a progressive and equitable city, a liveable and low carbon city, and a connected city. Through setting out a series of actions under the key themes, the document seeks to ensure that Manchester remains competitive internationally, predominately through the reuse of existing buildings and planned areas for business growth, whilst ensuring that this city is liveable through pursuing growth in new residential accommodation. Knott Mill can help to deliver these strategic objectives.

#### 3.3.2 MANCHESTER CORE STRATEGY 2012

The Core Strategy Development Plan Document 2012-2027 was adopted by the City Council on 11th July 2012. It is the key document in Manchester’s Local Development Framework. The Core Strategy sets out the long term strategic planning policies for Manchester’s future development.

Knott Mill has the potential to contribute to a wide range of Manchester’s strategic policy objectives set out within the Core Strategy and as summarised below:

**Policy SP1 Spatial Principles** sets out the key special principles which will guide development. It states that the focus of growth for the city centre will be for economic and commercial development, retail, leisure and cultural activity alongside high quality city living.

**Policy CC1 Primary Economic Development Focus:** states that the provision of a range of economic development uses, such as retail, leisure, entertainment, cultural and tourism facilities will be encouraged in the city centre, to support the development of a vibrant employment location attractive to businesses, employees and visitors to the city centre. The policy also states that this

location is considered a suitable location for high density buildings and commercially-led mixed use schemes.

**Policy CC2 Retail** seeks to promote the growth and improvement of retail provision in the city centre. Retail uses will be supported in the city centre where they would serve a local community or contribute to the areas character.

**Policy CC3 Housing** sets out that the city centre will see the most intense development of housing; setting a minimum target of 16,500 new units to be provided in the city centre up to 2027. One of the key areas identified for the residential development is Castlefield, adjacent to Knott Mill.

**Policy CC4 Visitors – Tourism, Culture and Leisure** states that the city centre will be the focus for culture and leisure in the City Region. Proposals to improve the appearance, use and accessibility of all cultural and visitor attractions and associated facilities will be supported.

**Policy CC5 Transport** seeks to maintain and enhance sustainable access to the city centre by all modes and to strengthen public transport capacity and quality.

**Policy CC6 City Centre High Density Development** confirms that city centre development will generally be high-density and that land should be used to maximise its efficiency.

**Policy CC7 Mixed Use Development** promotes mixed use developments as being most viable and able to contribute to regeneration initiatives within the city centre. The policy is supportive of developments that promote a range of uses, and particularly residential elements together with uses that support economic gain, for example active ground floor frontages that are occupied with retail, food and drink and leisure uses.

**Policy CC8 Change and Renewal:** The City Council has identified the types of development and approach to development which is considered most likely to deliver the vision for the city centre and development which reflects elements of this will generally be supported. The policy states that the city centre will welcome large-scale schemes and those that make significant contributions to the city centres role in terms of employment and retail growth, and more generally improve the social, economic or environmental well-being of Manchester.

**Policy CC9 Design and Heritage** expects the design of new buildings to be of the highest standard in terms of appearance and function. The policy states “development in Manchester city centre should preserve and enhance the heritage assets that have been identified, including listed buildings, conservation areas and scheduled ancient monuments”.

**Policy CC10 A Place for Everyone** aims to make Manchester a place that appeals to everyone.

**Policy EN2 Tall Buildings** states proposals for tall buildings will be supported where it can be demonstrated that they:

- Are of excellent design quality;
- Are appropriately located;
- Contribute positively to sustainability;

- Contribute positively to place making, for example as a landmark, by terminating a view or by signposting a facility of significance, and
- Will bring significant regeneration benefits.

**Policy H1 Overall Housing Provision** states that approximately 60,000 new homes will be provided for Manchester between March 2009 and March 2027, equating to an average of 3,333 units per year. There is a presumption towards high density housing development (over 75 units per hectare) within mixed use schemes which help contribute towards regeneration initiatives. The policy also states that 90% of residential development will be on previously developed land.

#### 3.3.3 RESIDENTIAL GROWTH STRATEGY (2016)

The Manchester Residential Growth Strategy represents an ambitious strategy to ensure that Manchester can build a minimum of 25,000 homes over the next decade. The Strategy builds on and updates the earlier Prospectus by setting out a clear vision to ensure that the city has the right housing mix in the right place to maintain its sustainability and successfulness as a ‘liveable’ city.

The Residential Growth Strategy sets six priority themes for housing to ensure Manchester has the right housing mix in the right place to support the city’s economy, alongside an implementation plan that identifies the following priorities over the short to medium term:

- i) Increase housing building on existing and new sites;
- ii) Improve the quality and sustainability of the city’s housing;
- iii) Increase opportunities for home ownership;
- iv) Expand the city’s family housing offer;
- v) Professionalise the private rented sector across the city; and
- vi) Provide appropriate housing options for retirement living.

A key aspect of this strategy is to reflect the broadening of the city’s economic base and the need for the city’s housing market to answer to both the new nature and scale of demand. The idea of this aspirational strategy is to provide opportunities to support high quality residential growth at sustainable locations. The Strategy states that specifically high density apartments will be prioritised in the city centre.

### 3.4 Regeneration Context

The strategic development of Knott Mill will also help achieve a number of regeneration objectives which are summarised below.

#### 3.4.1 CITY CENTRE STRATEGIC PLAN

On 2 March 2016, the City Centre Strategic Plan 2015 - 2018 was presented to the Executive Committee for endorsement.

The Strategic Plan includes a new city centre boundary that responds to the rapidly evolving economic geography of Manchester city centre. As both the economy and population of Manchester have grown, large scale mixed-use developments incorporating commercial, residential and leisure uses are driving change at its boundaries - including Great Jackson Street, First Street, and Castlefield to the south-west. The city centre boundary has therefore been extended to recognise the contribution of former 'fringe' areas and their relationship with the city centre.

The extension of the city centre means that Knott Mill sits well within the city centre and represents an important transition point between the existing and extended city centre.

Knott Mill can also contribute to a number of the future objectives for the city centre as set out in the Plan, including the following key aspirations:

- **Commercial** – Although the commercial market is buoyant, unless strategic action is taken, the impact of availability will see supply squeezed, prime rental rates increase and will limit the opportunities to satisfy large scale and footloose enquiries. As masterplans are developed for key areas of the city centre, space for commercial accommodation which is flexible and able to meet the needs of the market, will be central.
- **Residential** – Demand for city centre living that embraces different models and tenure types is rapidly increasing, and is a key attractor for a younger population who seek different lifestyle choices. The residential trajectory is expected to continue as a result of the continued increase in employment predicted in the city centre, as well as other factors such as higher levels of graduate retention. The city centre has benefited from long-established residential neighbourhoods such as Castlefield, the Northern Quarter and the Village. However, the need to identify and develop new city centre neighbourhoods is essential to satisfy existing and anticipated demand.
- **Retail** - As the UK's top performing retail destination outside of London there will be a continued offer and concentrated effort to support and diversify the existing retail offer in addition to facilitating new entrants into the city centre.
- **Leisure and the Evening Economy** - It is estimated that 150,000 people visit the city centre each weekend to enjoy the nightlife. The city centre and its night time economy are extremely important to Manchester.

Having a vibrant leisure and cultural offer is a key factor in people's choice about where they live, work and visit. It also contributes hugely to Manchester's economic growth.

#### 3.4.2 RELATIONSHIP TO SURROUNDING NEIGHBOURHOODS

The area to the south-west of the city centre is witnessing a significant surge in development activity and further development interest by virtue of its available land, highly accessible location, unique heritage character, supportive planning policy and land assembly. There is substantial construction activity and a substantial development pipeline through grant of various planning permissions.

New development is coming forward in a coordinated and cohesive way based on adopted Strategic Development Frameworks for various areas around Knott Mill, in particular at Great Jackson Street and First Street. These developments have planned for and delivered vibrant mixed-use neighbourhoods; promoted permeability; considered heritage assets; and mediated scale and density. A number of permitted tall buildings, many of which have been built or are being built, have established a context of increased density and scale in this part of the city centre. This has further emphasised the extension of the city centre into this previous 'fringe' location.

Knott Mill will directly interface with these developments and has the potential to knit together the predominantly residential area of Great Jackson Street, and the mixed-use but commercially focused First Street area. Indeed, Knott Mill embodies the final piece of the city centre's south western regeneration jig-saw. This emphasises the importance of the permeability of the Knott Mill area and connectivity to its neighbours. Together, these new and improved neighbourhoods deliver a multitude of regeneration benefits.

The redevelopment of Knott Mill is an important next step in the evolution of this part of the city centre. A mix of uses encourages the creation of a dynamic, lively locale. Protection of buildings of architectural and heritage merit ensures the area's connection to its past is maintained. Increased density and scale at carefully selected locations mirrors and complements changes to adjacent neighbourhoods.

### 3.5 Conclusion Role of Knott Mill

Given its location, the existence of a number of development-ready sites and its proximity to key transport nodes, the south west of the city centre will play a significant part in satisfying current and future demand for new dwellings and commercial space in new neighbourhoods, which is required in order to support population and economic growth.

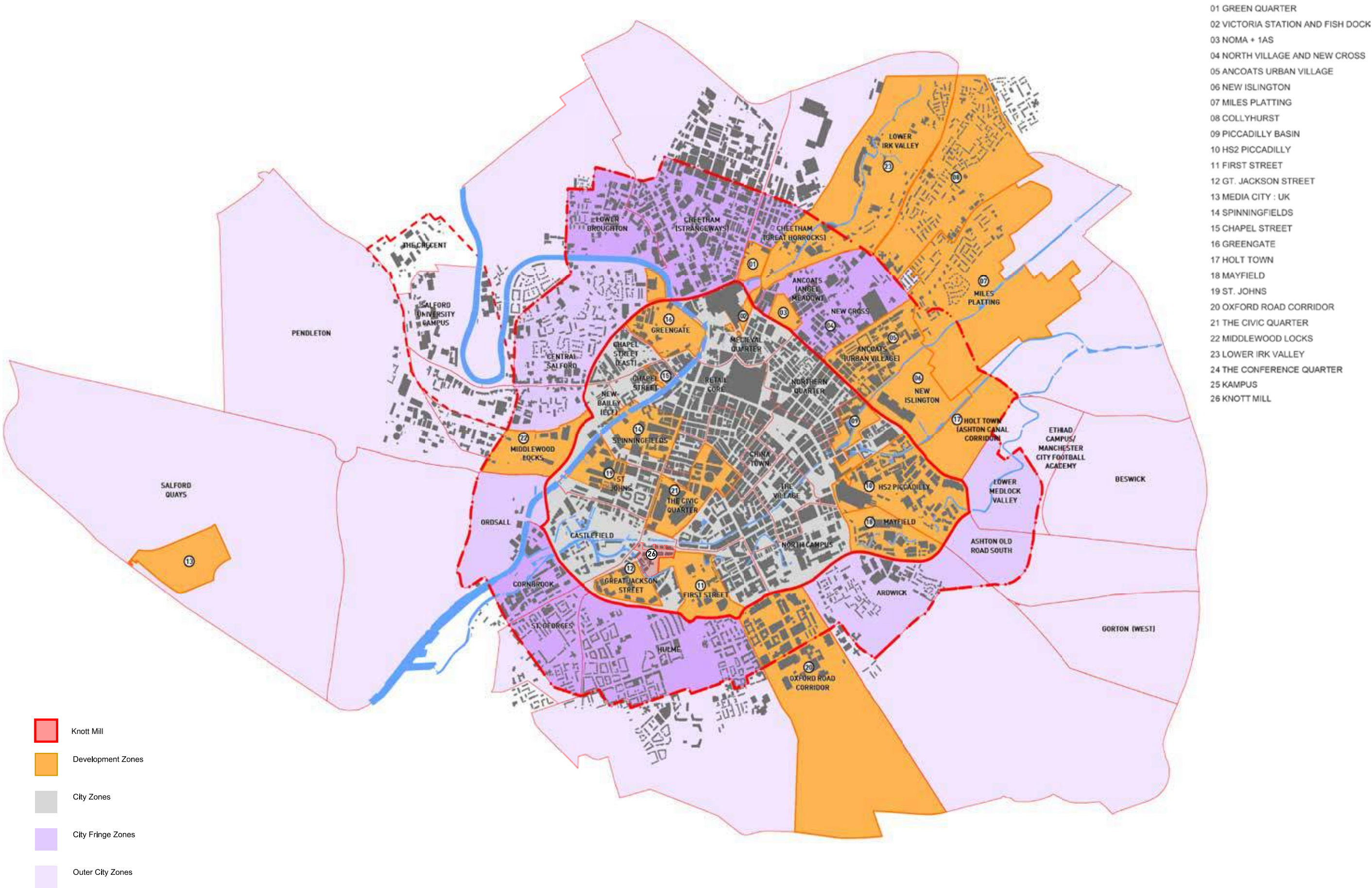
A number of neighbourhoods located within Manchester's south western city centre edges have already been the focus of significant regeneration activity. Strategic planning policy and regeneration guidance is being developed to provide frameworks for the transformation of these areas into vibrant new mixed use neighbourhoods of choice such as at Great Jackson Street and First Street. Knott Mill represents an opportunity to knit together these larger areas and plan strategically for how its unique characteristics and circumstances can contribute to the continued growth of this part of the city centre.







4.0 STUDY AREA APPRAISAL





## 4.1 The Framework Study Area

### 4.1.1 OVERVIEW

The focus of the Masterplan is the Knott Mill area of Manchester, located on the southern fringe of the city centre. The Study Area is irregular in shape and broadly bound by Hewitt Street and the Deansgate railway viaduct to the north, Albion Street and the Little Peter Street car park to the east, the River Medlock to the south, and the historic, southern extent of (old) Deansgate to the west. The Study Area extends slightly beyond these broad boundaries at Bugle Street, Rowendale Street, and Cameron Street to the north and it connects to Chester Rd/Whitworth St to the north west. The Study Area comprises a mix of relatively low-rise commercial and residential uses. Several, primarily professional and creative sector industries currently operate within the Study Area including architectural practices, graphic designers, digital marketing agencies and property developers.

### 4.1.2 ADJACENT FRAMEWORKS OVERVIEW

The Study Area is surrounded by the distinct Strategic Regeneration Frameworks (SRFs) of Great Jackson Street to the south, First Street to the east, Whitworth Street West to the north (with the Great Northern, Manchester Central and Castlefield Quay further to the north), and the Castlefield Conservation Area to the west. The Study Area is excluded from these framework areas but has the potential to play a pivotal role in connecting all these areas to each other and with the city centre.

### 4.1.3 CREATIVE RIBBON

The First Street Development Framework introduced the concept of an east-west 'Creative Ribbon' between Oxford Road and Little Peter Street in order to fully integrate First Street into the city centre. An opportunity was identified to build on the existing cluster of cultural and creative businesses around Knott Mill, Whitworth Street West and the HOME cultural building to create a new distinctive cultural district of the city centre.

### 4.1.4 GREAT JACKSON STREET

The Great Jackson Street Strategic Regeneration Framework, originally adopted in 2007 and recently updated, seeks to create a new high-density residential-led quarter of Manchester. It has good connections to the City centre and to other local amenities such as Hulme Park, First Street, and the Castlefield Basin. The ambition is to create a vibrant, safe, secure and sustainable community with a range of dwelling types that provide an attractive place to live, ranging from one, two and three bed apartments to townhouses and retirement apartments.





## 4.0 STUDY AREA APPRAISAL

### 4.1 The Framework Study Area

#### 4.1.5 FIRST STREET

The updated First Street Strategic Regeneration Framework (SRF) was adopted in 2015. First Street is located at an important point of entry into the city centre from the airport and from the south. The SRF is primarily concerned with creating “a sense of place” to help it become the heart and commercial focal point of a wider neighbourhood. The SRF proposed a commercially-led mix of uses, targeted at growth markets such as ICT, healthcare and R&D. First Street North presently comprises the HOME cultural building, a 4\* hotel, public multi-storey car park, large commercial office buildings at No 1 and No 8 First Street and retail and food and beverage outlets.

The 2018 Framework updates the development principles for First Street Central. First St Central will remain the focus for commercial office development within the masterplan. First Street is now an established commercial office location, with an ecosystem of high quality occupiers emerging within No.1 and No.8 First Street. In order to capitalise on this popularity and respond to ongoing demand for Grade A office floorspace in the City Centre, a higher scale and density of office accommodation in the central part of the masterplan is now feasible. This could be combined with additional hotel floorspace, which would support Manchester’s continued growth in popularity as a visitor destination, and add to the offer at First Street.

#### 4.1.6 GREAT NORTHERN, MANCHESTER CENTRAL AND CASTLEFIELD QUAY

The Great Northern, Manchester Central and Castlefield Quay Strategic Regeneration Framework was adopted in 2017 to guide the future re-configuration, re-development and re-positioning of the three interlinked city centre sites. The Great Northern site is to be regenerated so that it fulfils its potential as a cultural, business and residential destination. Manchester Central proposals comprise an opportunity to create taller buildings which complement the adjacent Beetham Tower. Castlefield Quay provides an opportunity to create a mixed-use development and promote the regeneration of the currently dilapidated viaduct arches.

#### 4.1.7 WHITWORTH STREET WEST

The key aim of the Whitworth Street West Framework is to advance the area’s destination status through development and to improve its overall reputation, profile and legibility. The Framework states that the key junctions of Medlock Street, Albion Street and Chester Road/Deansgate offer a unique opportunity for the development of landmark structures that maximise the beneficial impact of important vistas, enhance the visitor’s sense of arrival, terminate key views and provide visual connectivity with the core of the city.

The Framework area considers the sites between Whitworth St West and the railway viaduct as suitable for large scale redevelopment. Proposals have already come forward for 10-12 Whitworth St West and it is understood that development proposals will be forthcoming for 2-8 Whitworth St West and for the Deansgate Station site.



Great Jackson Street Framework



First Street Framework

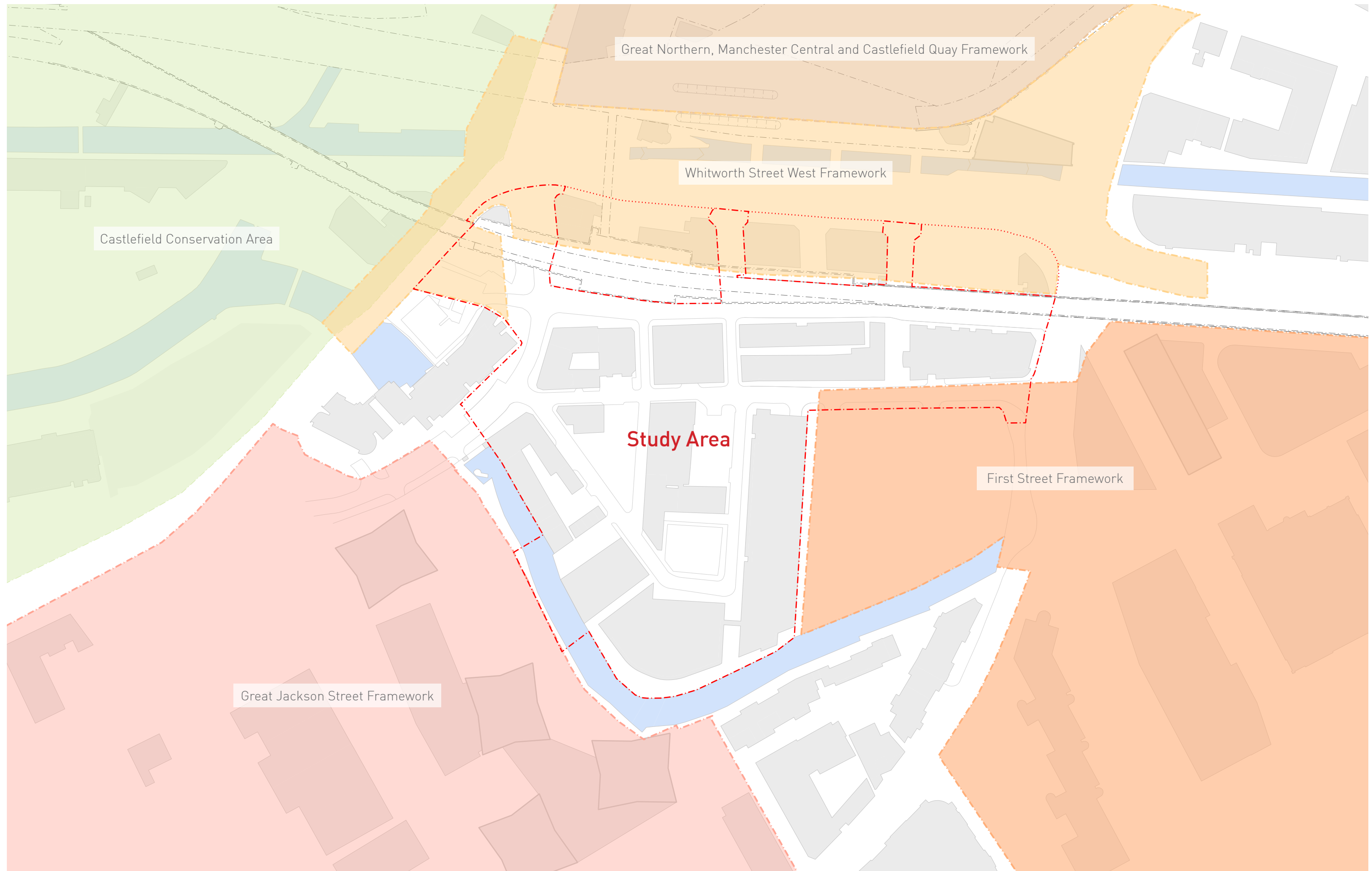


Great Northern, Manchester Central and Castlefield Quay Framework



Whitworth Street West Framework





4.0 STUDY AREA APPRAISAL

4.2 Site History

4.2.1 WIDER INDUSTRIAL DEVELOPMENT - RIVER AND CANALS

Knott Mill is of great significance to two periods of history, namely the Roman and Early Modern Industrial Periods.

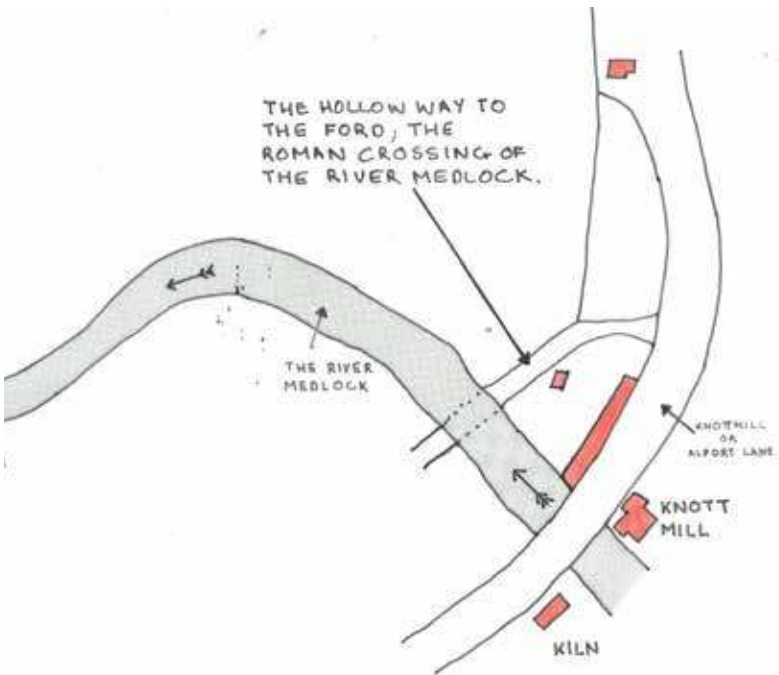
The Roman fort in Manchester was founded in the last quarter of the First Century AD and the line of the Roman road south from the fort lies alongside the Study Area.

In 1759 construction of the Bridgewater Canal, which was initially built to bring coal from the Duke of Bridgewater’s mines at Worsley to Manchester, began. The canal terminates end on to the River Medlock from which it was able to obtain a direct supply of water. The choice of the site will have been influenced by the fact that the main road through Manchester passed over the adjacent Knott Mill bridge. The Duke’s Warehouse was also built in this location parallel to the road and spanning the canal.

4.2.2 WIDER INDUSTRIAL DEVELOPMENT - BUILDINGS

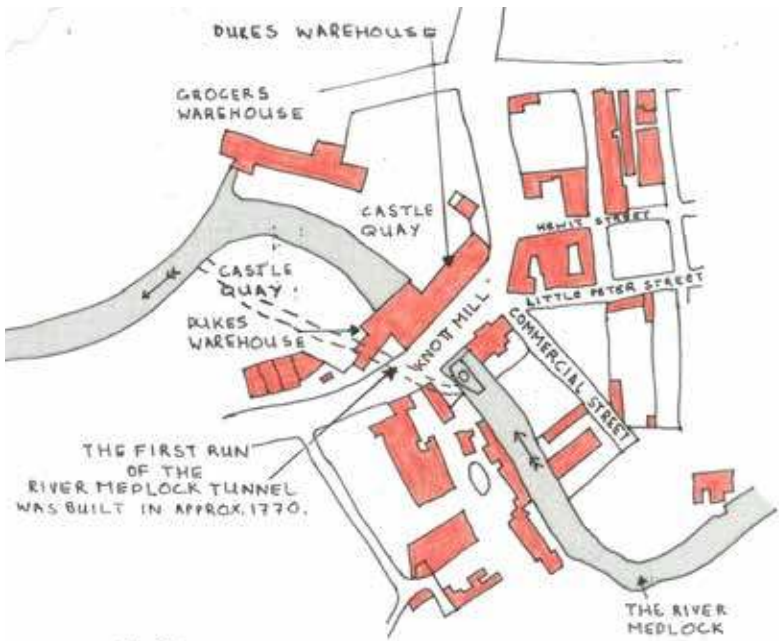
The Castlefield Basin expanded with the construction of wharves, warehouses, stables, workshops and lime kilns. At the end of 1804, the Rochdale Canal was extended through Manchester to Castlefield where it joins the Bridgewater Canal. Castlefield became one of Manchester’s first industrial complexes.

By 1794, at Castlefield and Knott Mill, there were flour dealers, a coal wharf, vitriol manufacturers, dyers, cotton manufacturers, salt and corn warehouses and a log mill. Canal warehouses became essential for the collection, storage and security of goods. They were, in effect, transfer sheds for goods between canal and road networks.



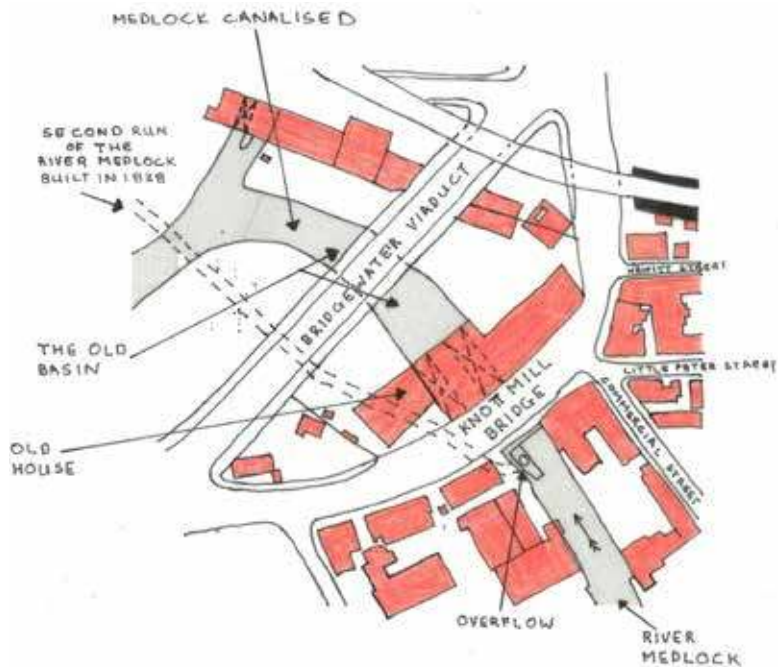
1765 - RIVER AND CANAL DEVELOPMENT

Knott Mill / Alport Lane crossed the River Medlock via Knott Mill Bridge. This was one of the main routes through Manchester.



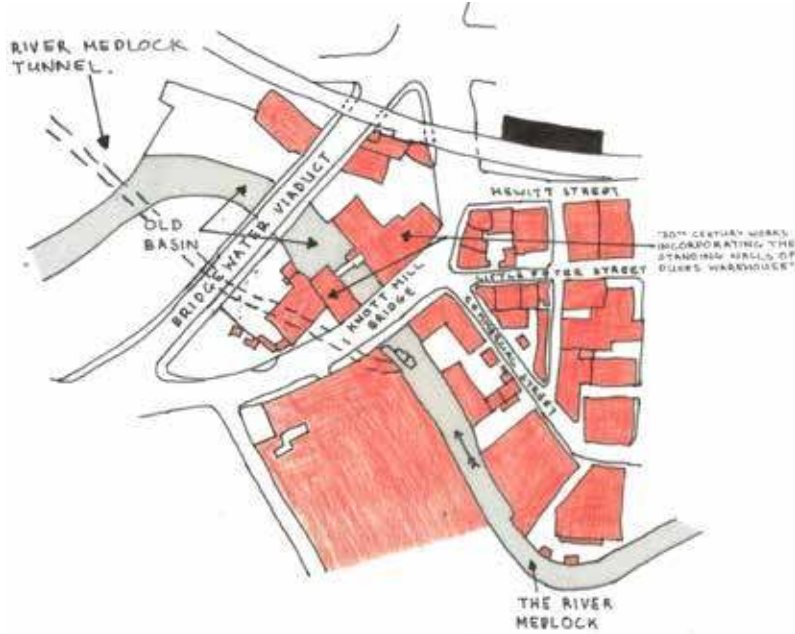
1794 - RIVER AND CANAL DEVELOPMENT

Records indicate that it is likely that the first run of the River Medlock tunnel was built in approximately 1770. Proceeding this, the Duke’s Warehouse was built over the river.



1851 - RIVER AND CANAL DEVELOPMENT

Records indicate that it is likely that the second run of the River Medlock tunnel was built in 1828. The tunnel comes out at Giants Basin, allowing for greater control in regulating the water level for the warehouses built further downstream.

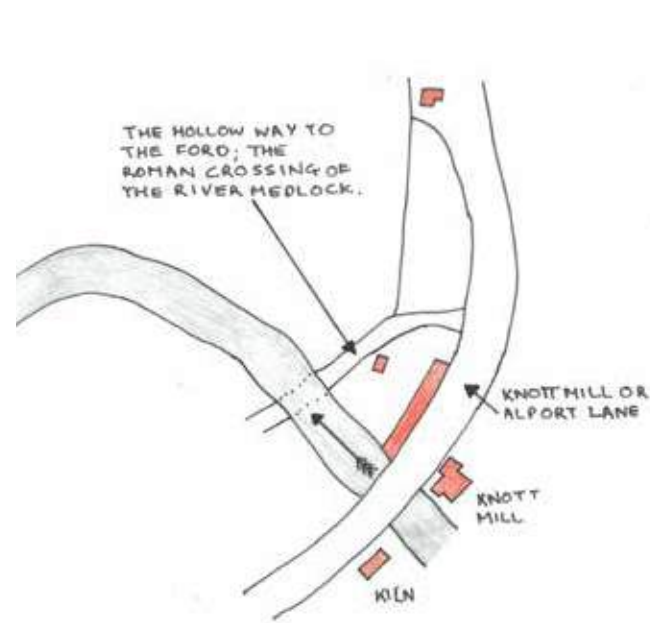


1965 - RIVER AND CANAL DEVELOPMENT

Little change to the canal system as its use as a method of transporting goods diminished.

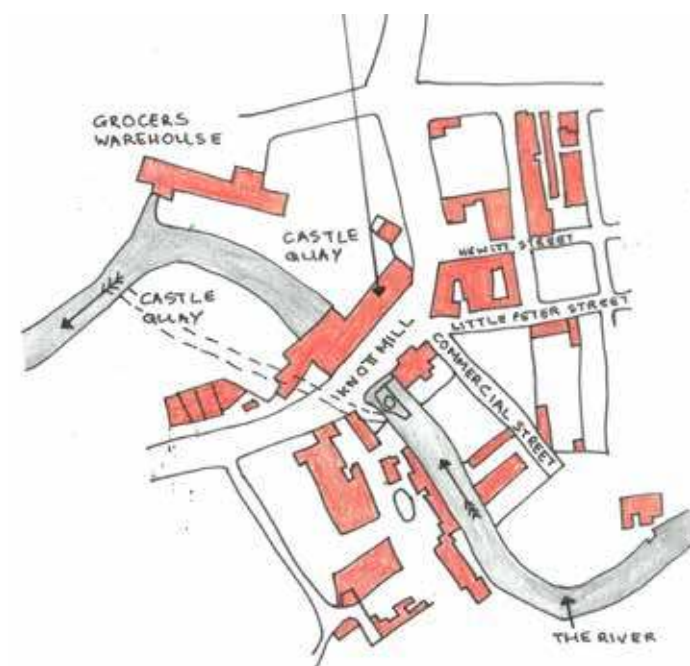
Historic development and evolution of the local river and canal network





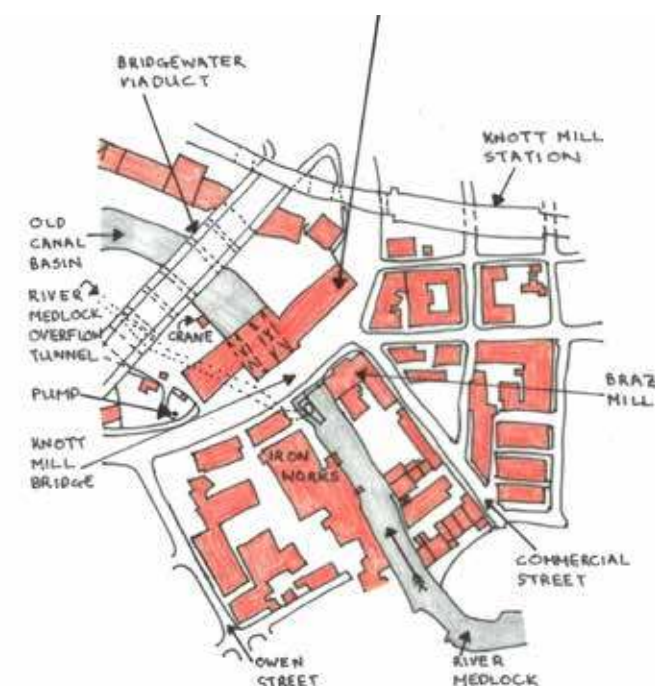
#### 1765 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

The map shows the Hollow Way to the Ford which was the Roman crossing of the River Medlock.



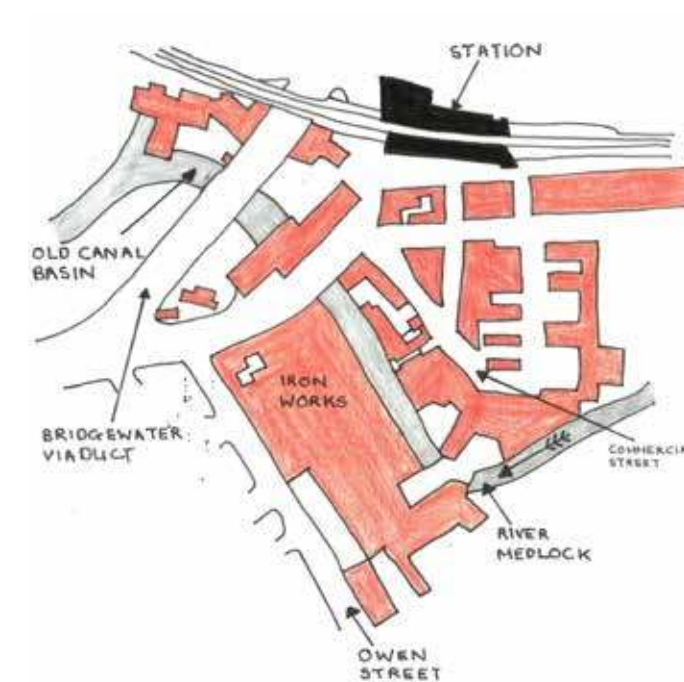
#### 1794 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

The significant development in the area is in response to construction of the canal network and basin.



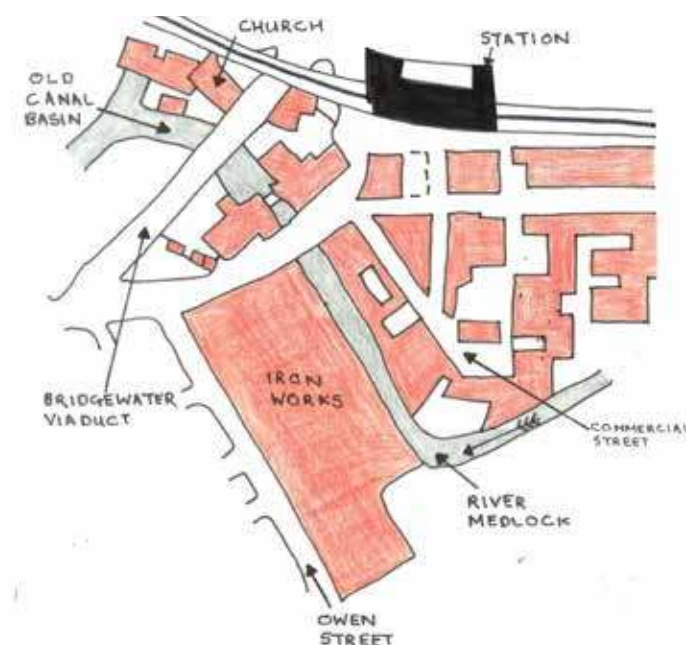
#### 1849 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

The infrastructure of the area had been developed significantly, including the construction of the River Medlock overflow tunnel, the bridgewater viaduct and the railway line to Knott Mill Station.



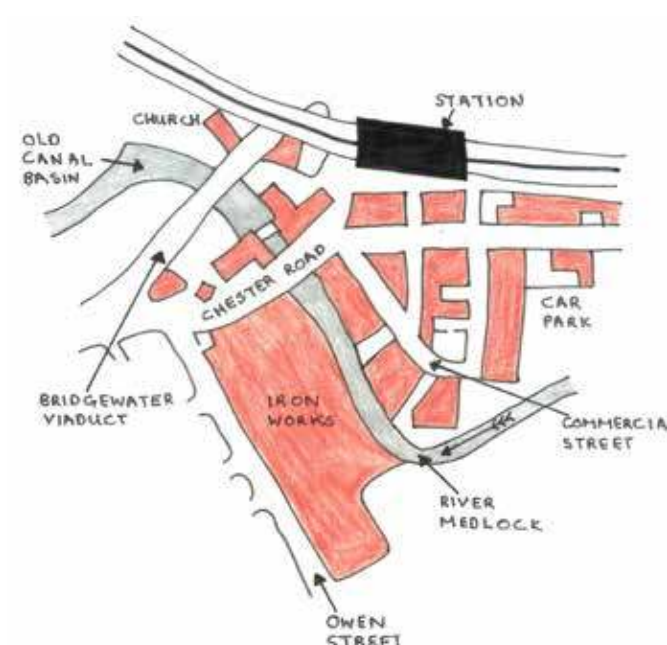
#### 1923 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

The Dukes Warehouse was damaged by a fire in 1919, the building in this map may be the shell of the damaged warehouse.



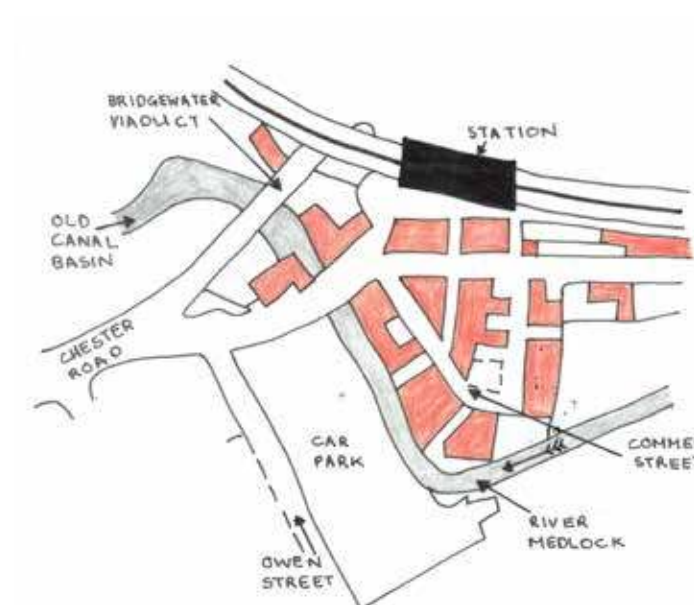
#### 1959 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

Further decline in industrial uses and beginnings of erosion of the urban grain. The Duke's Warehouse is shown as two separate buildings. Both appear to have new sections added.



#### 1972 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

Further demolition and increase in vacant plots. Remainder of former industrial buildings now largely derelict.

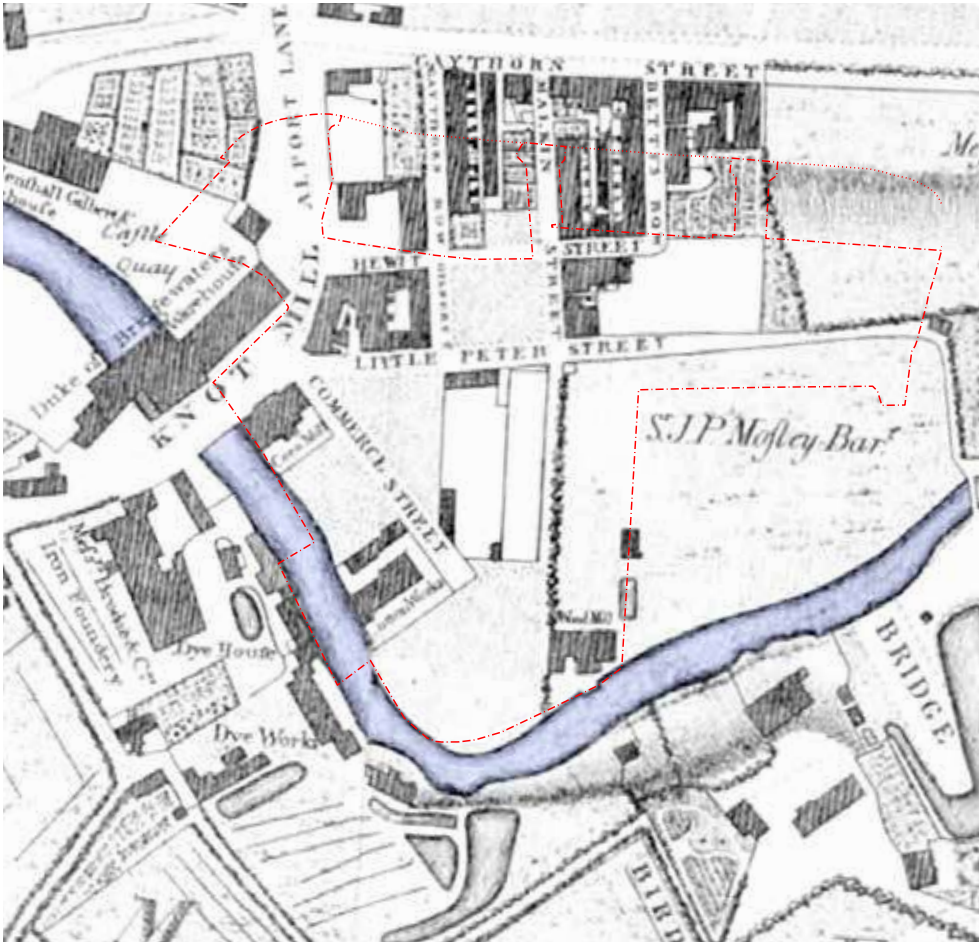


#### 1989 - DEVELOPMENT OF INDUSTRIAL BUILDINGS

The smaller buildings on site were demolished and yet further demolition of some larger buildings which has eroded the street edge definition in places. Boardwalk venue and some commercial uses begin to inhabit vacant buildings.



4.0 STUDY AREA APPRAISAL



1787 - 1794

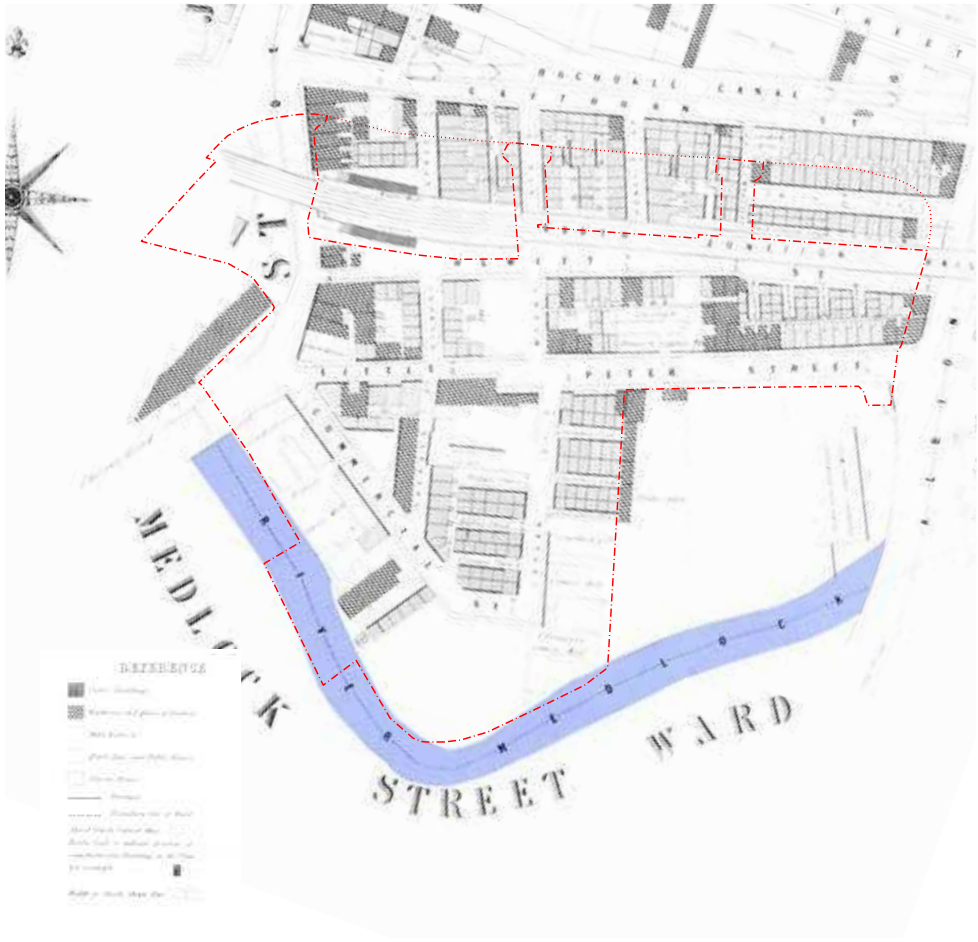
By the 1780s the national demand for textiles, particularly cotton, began to rise, resulting in a dramatic increase in mill building that transformed Manchester. The process of industrial development was facilitated greatly by the introduction of canals which provided the first efficient means of transporting bulk loads of goods.

The 18th Century saw steady development which consisted mainly of mills and associated workers' housing and later for warehousing and other industrial works. At the late 18th Century, the central, southern and eastern extents of the study area remain largely undeveloped.

Construction of the Bridgewater Canal led to the encroachment of industrial development emerging from the area around Knott Mill bridge. Commercial Street (then known as Commerce St) extends to open ground at its south eastern extent with a cotton works shown adjacent to the River. Beyond, at the south eastern extent of the study area, a building know as Wood Mill also sits adjacent to the River.

Some enclosure and building work has been undertaken between Commerce Street and Little Peter Street with the line of the present Jordan Street defined by a hedge line. The enclosure work undertaken around this time defined the street pattern which remains largely unchanged to the present day.

Industrial



1794 - 1851

The industrial boom of the era is apparent in the change in the character of the area. The railway line and viaduct has been constructed to the north of the study area, Knott Mill and the surrounding areas are now heavily urbanised with dense development blocks.

By this time, the Corn Mill known as Brazil Mill adjacent to the River and Knott Mill Bridge had expanded forming an L-shape plan and was converted for use as a textile mill.

Jordan Street has now been laid out alongside the development abutting Little Peter Street which was known as Jordan's Cotton Mill. The industrial activity around the former Wood Mill (known in 1851 as Chappel's Cotton Mill) has expanded north along Jordan Street and comprises a complex known as Leeds and Co's Cotton Mill.

Between 1831 and 1851, blocks of back to back workers housing has been developed around Alpha Place and around the junction of Jordan St and Little Peter St.

Commerce Street is now recorded as Commercial Street and Creaves St (now known as Constance Street) has been laid out and defined by new development along its length turning west along Little Peter St.

Industrial / Residential



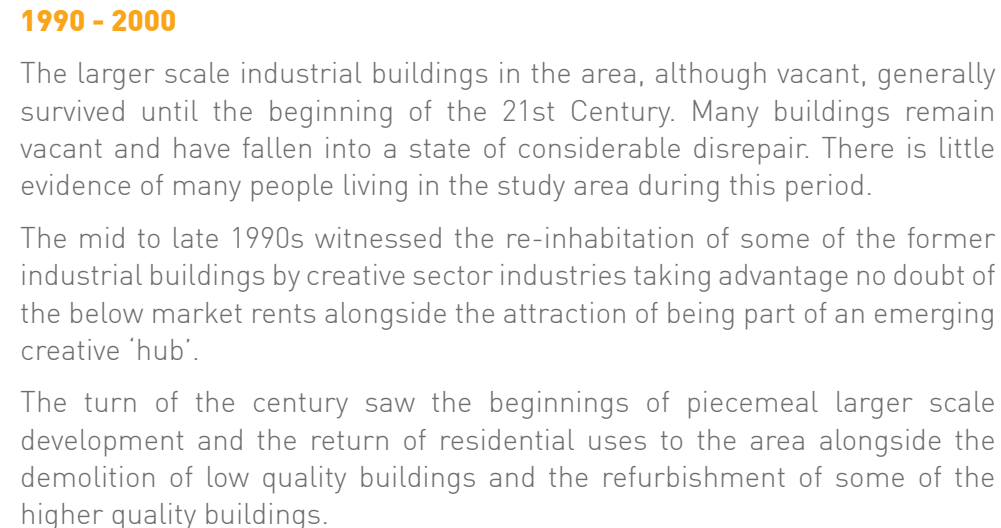
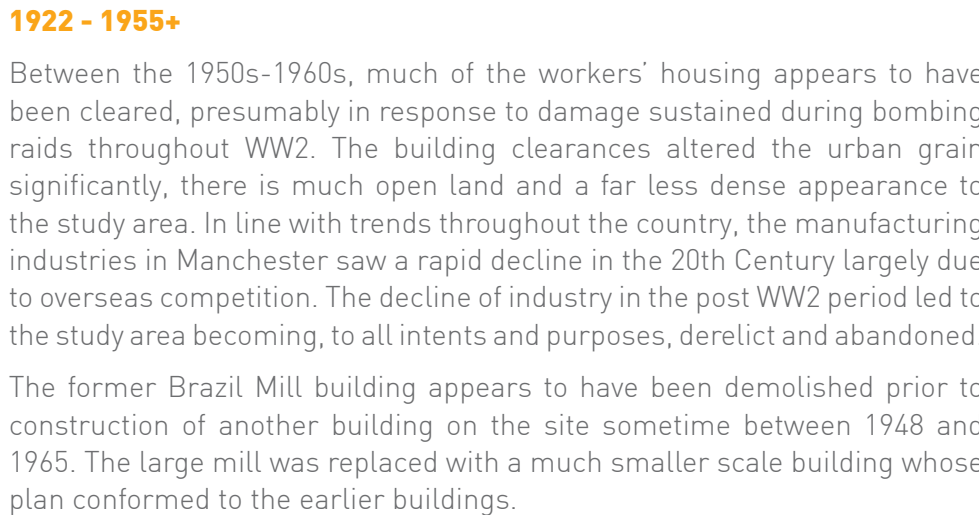
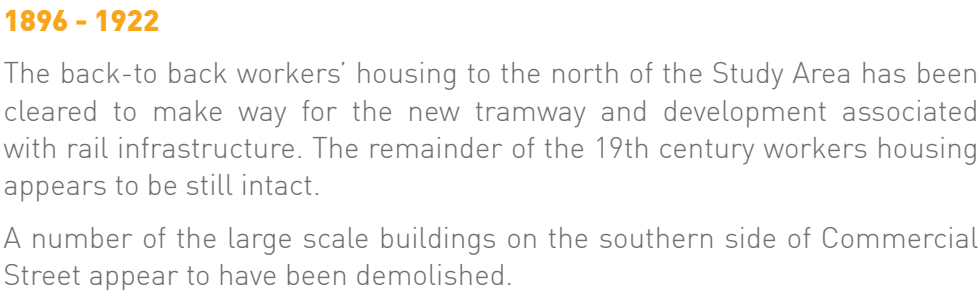
1851 - 1896

Towards the end of the 19th Century further expansion of the back-to-back workers housing took place together with a continued increase in the density of development within the study area.

By 1865, it seems the Brazil Mill complex had fragmented into multiple occupancy and comprised numerous textile based trades such as smallware manufacturers, an umbrella manufacturer, a wheelwright, a machine maker and a screw bolt maker.

Industrial / Residential







## 4.0 STUDY AREA APPRAISAL

### 4.2 Site History

#### 4.2.3 HISTORICAL DEVELOPMENT OF KNOTT MILL PRE 1975

The Study Area benefits from a detailed and layered historic context which has been critical in informing the development of the vision and Masterplan for Knott Mill.

The onset of rapid industrialisation in the 19th Century saw Manchester become one of the fastest growing cities in the world. The national demand for textiles, particularly cotton, began to rise, resulting in a dramatic increase in mill buildings that transformed Manchester into a centre of the factory-based cotton manufacturing industry.

This process of industrial development was facilitated greatly by the introduction of canals, which provided the first efficient means of transporting bulk loads of goods. During the construction of the canals, a channel was cut from the River Medlock to allow water to flow through the industrial complex at Knott Mill.

As part of the industrialisation process, Knott Mill also saw the introduction of rows of workers housing occupying spaces between closely-packed factories, warehouses and other commercial premises.

The Manchester Local Image Collection photographs adjacent highlight the rows of back to back workers' housing which sat in close proximity to the industrial buildings. Of particular note is the reduction in scale and density within the study area which is particularly evident in the two comparative views of Commercial Street taken 75 years apart.



Knott Mill Easter Fair 1877



Bugle Street 1896



Brazil Mill / Commercial Street 1899



Hewitt Street / Gilbert Street 1907





Little Peter Street 1973



Gilbert Street 1907



Looking Towards Little Peter Street 1973



Brazil Mill 1899



Commercial Street from Jordan Street 1973



Former St Peter's School 1973



## 4.0 STUDY AREA APPRAISAL

### 4.2 Site History

#### 4.2.4 HISTORICAL DEVELOPMENT OF KNOTT MILL 1975-1990

Following the decline of industry in the mid-20th century, the area fell into a cycle of decline which saw it become under-used and largely derelict.

The 1980s and 1990s saw the partial revival of the Knott Mill area which became a hub for creative industries who were initially attracted by the low rents that an area in decline could offer. The area soon became host to a distinctive music and cultural scene that developed in Manchester in the 1980s.

The music scene was dominated by The Smiths, New Order and The Fall. An initiative of Factory Records, the Hacienda nightclub opened nearby in 1982. This musical backdrop became a significant influence on the ‘Madchester’ scene - a music and cultural scene that merged alternative rock with acid-house culture and other influences such as psychedelia and 1960s pop. The ‘Madchester’ label was popularised by the British music press in the early 1990s and included groups such as the Happy Mondays, the Stone Roses, the Inspiral Carpets, Northside, 808 State, James, and The Charlatans. As the ‘Madchester’ scene dwindled, the Britpop scene emerged with the likes of Oasis finding prominence.

This musical and popular cultural setting saw a mushrooming of Manchester’s nightlife and a burgeoning media and creative industry sector which has had a long-term impact on the development of the City and the Knott Mill area specifically.

The archive photographs adjacent were taken around 1991 and capture the area in decline. The Packhorse Pub and The Boardwalk venue sat amongst scattered light-industry and provided the only destinations for those crossing the southern extent of this part of the city centre.



Aerial View of Knott Mill circa 1990



View Along Little Peter Street 1991





View Along Hewitt Street 1991



View Along Constance Street 1991



View Along Hewitt Street 1991



View Towards Deansgate Station 1991



Little Peter Street 1991



## 4.0 STUDY AREA APPRAISAL

### 4.2 Site History

#### 4.2.4 CENTRAL MANCHESTER DEVELOPMENT CORPORATION KNOTT MILL AREA DEVELOPMENT STRATEGY 1992 OVERVIEW

In the 1990s, Knott Mill was the focus of a masterplanning exercise which saw efforts to revive the area through a workable urban design strategy. The work was instrumental in rejuvenating the area and facilitating the movement of many creative industries to the Knott Mill area. Over time, the area transformed into a creative hub, popular amongst architects, designers, photographers and musicians.

As a result of the strategy's implementation, the area saw the delivery of environmental improvement works and an enhanced streetscape through public realm improvements and works to the pavements and roads, assisted by grant funding from the Central Manchester Development Corporation.

##### 4.2.4.1 PARTICIPATION IN THE EARLY MASTERPLAN STAGES

Participation in the masterplanning exercise was actively encouraged with several benefits cited including: giving Knott Mill a voice; integration and assimilation of business communities' objectives and aspirations; capitalisation on changes in-and-around Manchester; the targeting and collection of grant aid; and to counter the threat of Compulsory Purchase Order action.

Landowners were advised that their participation in the exercise would result in several benefits including:

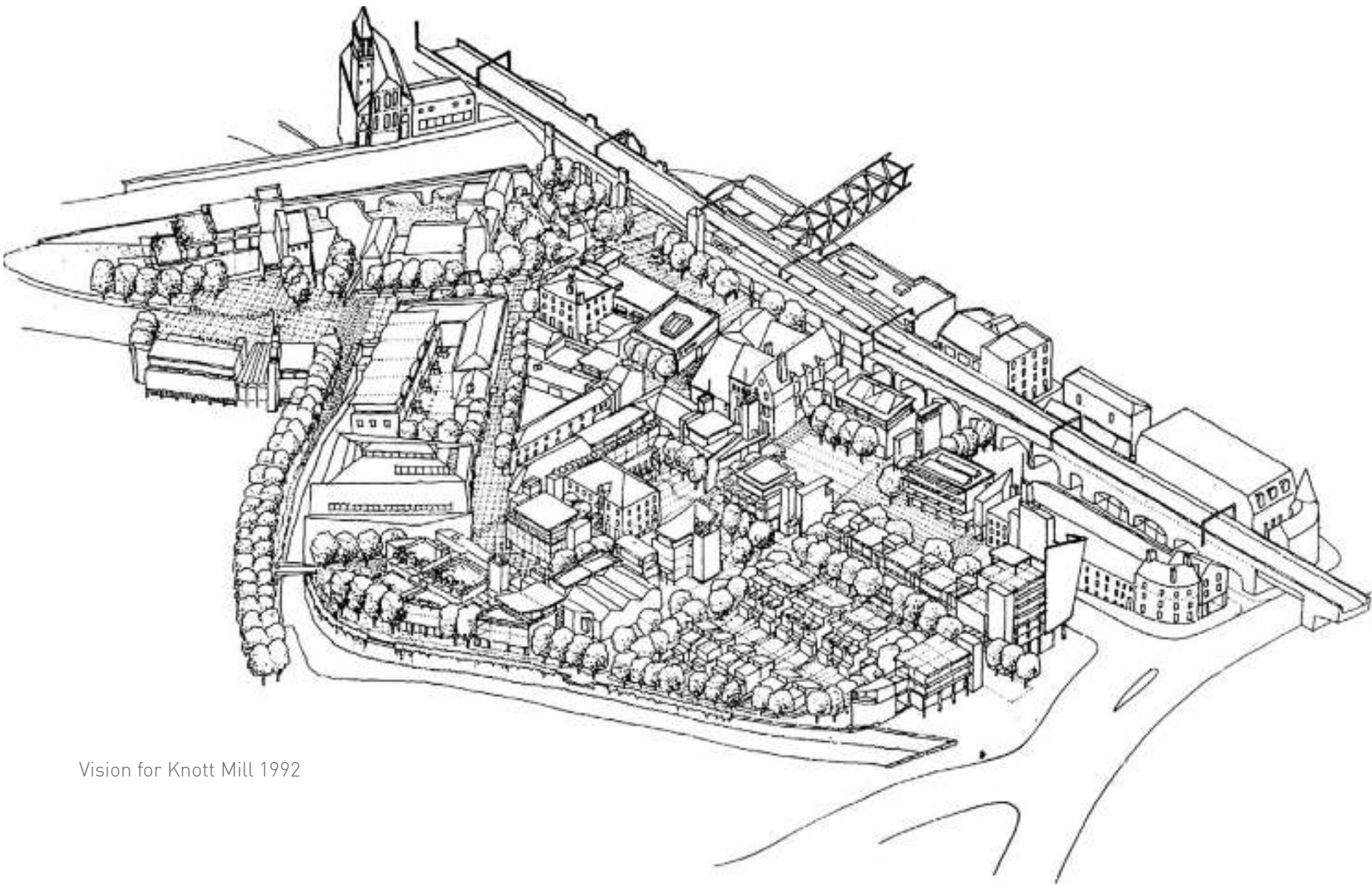
- Guidance and advice on: existing land values; an outline structural appraisal of individual properties; and an outline of possible uses within a strategy plan;
- A voice in a planning appraisal which would carry weight with the Central Manchester Development Corporation;
- A specific, purpose-produced report for Knott Mill;
- A presence as part of a single application for grant aid; and
- Access to a local team of consultants well acquainted with Knott Mill and its potential.

##### 4.2.4.2 FUNDING

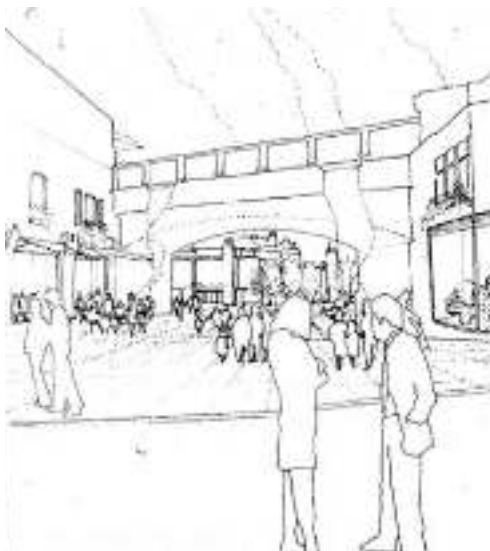
Funding for the delivery of the Masterplan was available by way of an Environmental Improvement Grant (EIG). Through this grant, funding for 50% of project costs were available. The EIG was considered to be less complicated than other forms funding, with fewer constraints and no repayment provisions.

The EIG was typically available for building cleaning and general environmental improvement works including landscaping and tree planting.

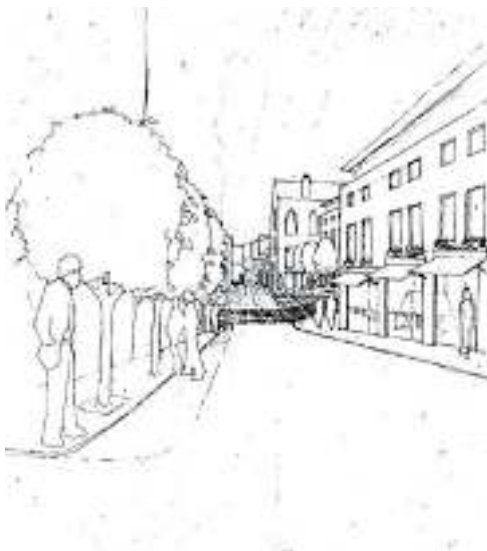
In addition, a City Grant was also available for development schemes throughout the Development Corporation Area. Funds were provided on a funds deficit calculation between cost and value, which roughly equated to one



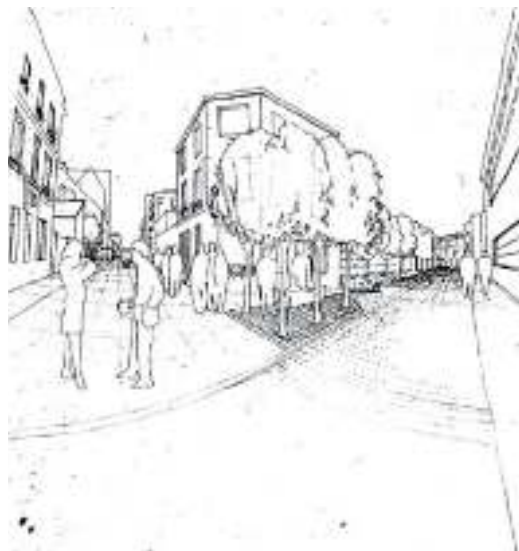
Vision for Knott Mill 1992



Gateway to Knott Mill



View of Little Peter Street



View of Commercial Street



View Along Constance Street



## 4.2 Site History

part public money to four parts private money. The proposals were discussed and agreed with landowners and a commitment made to contribute to the funding of the works.

### 4.2.4.3 THE 1992 MASTERPLAN - FACELIFT

General cleaning and upgrading of the existing building stock was proposed, with a gradual removal or repair of redundant structures and building fabric.

### 4.2.4.4 THE 1992 MASTERPLAN - SIGNAGE AND LIGHTING

Signposting the principal gateways was seen as an important way of raising public awareness of Knott Mill. In this way, further signage within the area around and adjacent to the riverside walkways was seen as a way of linking Knott Mill with Grand Island (now First Street), and Castlefield.

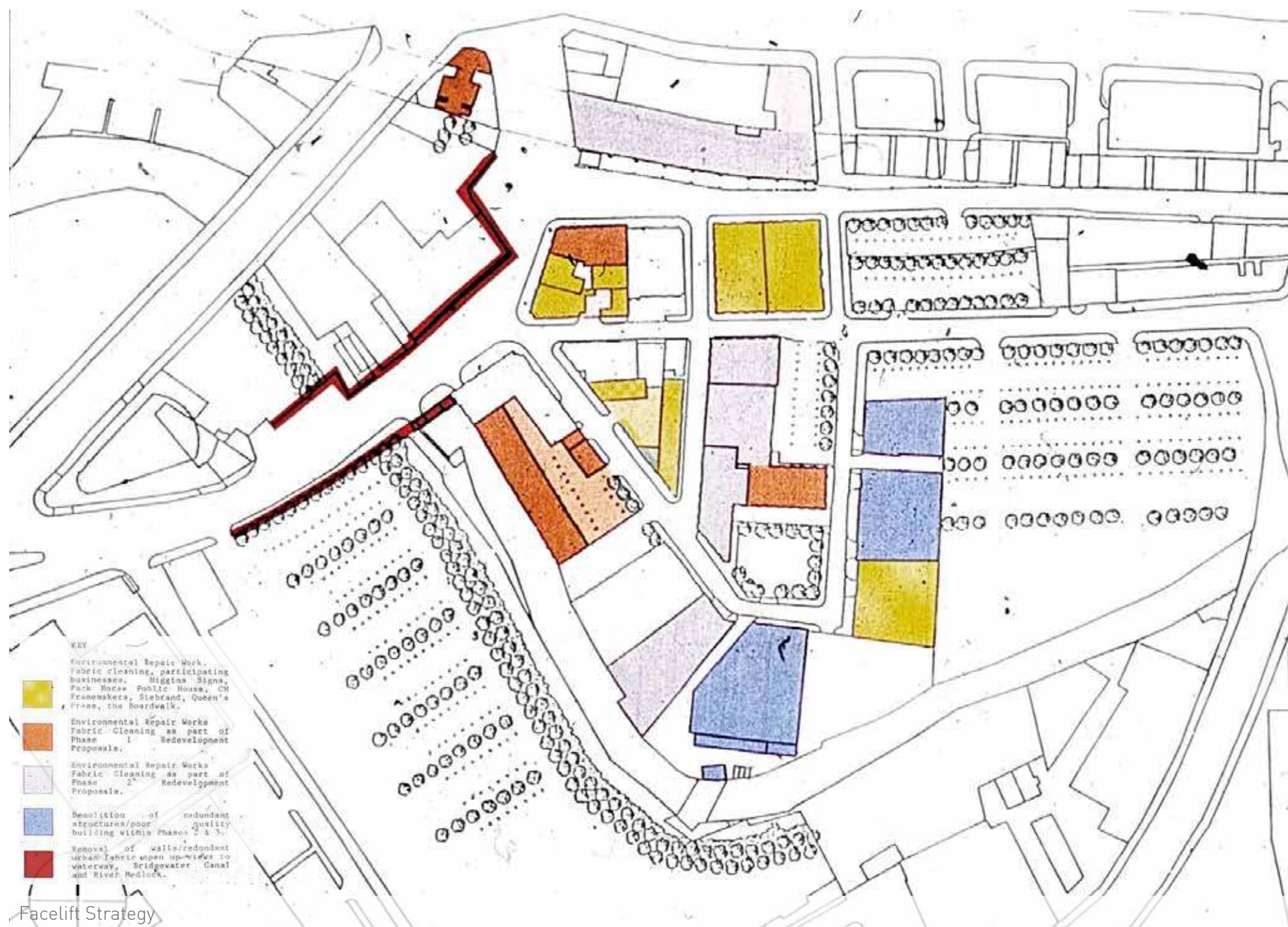
An opportunity was identified for upgrading the existing street lighting to provide an appropriate level of illumination within the area, and also improve at least the perception of safety after dark. It was considered key buildings and structures should be floodlit in the evenings to highlight the diversity of the architecture within Knott Mill with a specific focus on floodlighting the rail viaducts and Deansgate Station, The Boardwalk and the buildings located along the River Medlock.

### 4.2.4.5 THE 1992 MASTERPLAN - TRAFFIC CIRCULATION

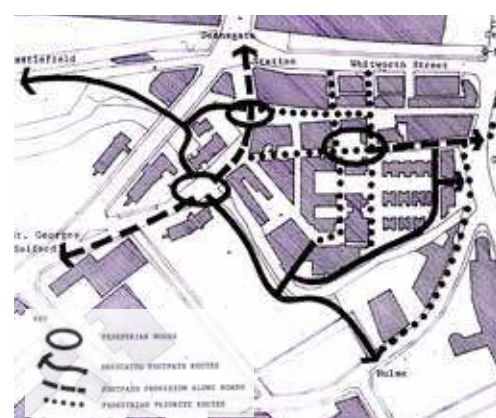
The importance of good road connections to the business community was also recognised. Proposals aimed to provide a traffic-calmed central area including the reduced importance of Little Peter Street which was seen as a mechanism to reduce through-traffic and increase access clarity from more peripheral routes. It was also suggested that an extension of Rowendale Street to the River Medlock would provide a simple clear access to the heart of Knott Mill.

### 4.2.4.6 THE 1992 MASTERPLAN - CAR PARKING MANAGEMENT

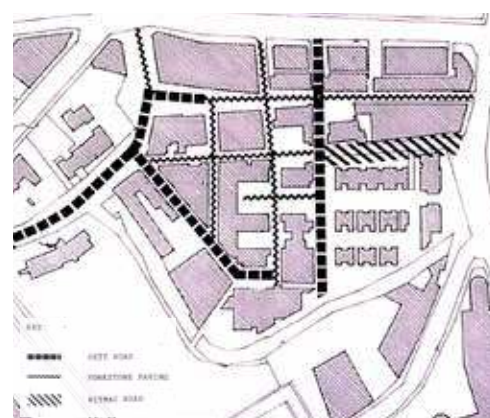
The Knott Mill area was originally planned as a major provider of car park space within the city centre area with general meter parking retained at Little Peter Street. Proposals included the introduction of 'pay and display' street parking within the core area allowing priority to local business users. Further, the extension of road access within Knott Mill was seen to allow additional private off-street parking throughout the area.



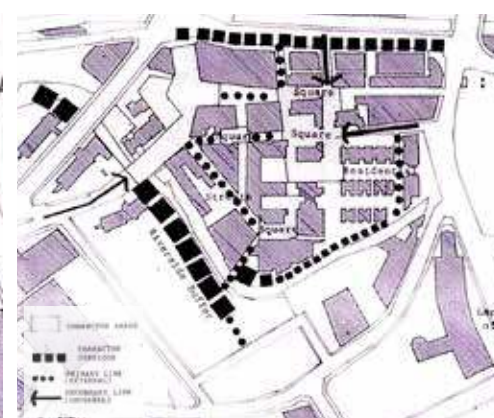
Facelift Strategy



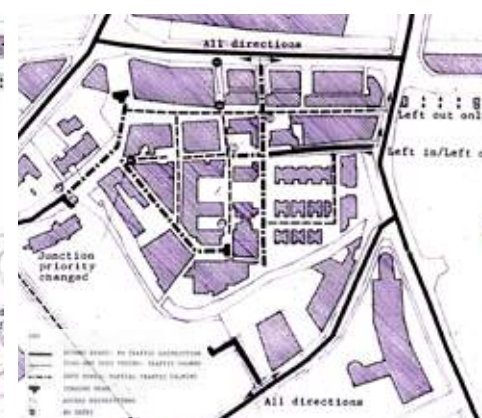
Pedestrian Strategy



Streetscape Strategy



Landscape Strategy



Traffic Circulation Strategy



# 4.0 STUDY AREA APPRAISAL

## 4.2 Site History

### 4.2.4.7 THE 1992 MASTERPLAN - PROPOSED USES

A wide mix of uses was proposed as part of the masterplan including: studios/offices; commercial offices; retail/workshops; leisure/bars/restaurants; and residential.

### 4.2.4.8 THE 1992 MASTERPLAN - PEDESTRIAN CIRCULATION STRATEGY

Increased pedestrian access was seen as an important element of the revitalisation of Knott Mill as increased usage by pedestrians was considered to enliven streets, making them more attractive and safe.

In 1992, pedestrian through-routes largely by-passed Knott Mill. The masterplan strategy therefore proposed to: connect footpath routes into Knott Mill; reduce through traffic, creating a safer realm for pedestrians; and improve potentially important, city-wide paths.

The strategy aimed to connect Castlefield with Grand Island (now First Street) along the River Medlock through the establishment of a riverside walk and link, via a footbridge across the River Medlock. Proposals also aimed to improve roadside footpath links along major peripheral roads, particularly connecting to Hulme along City Road.

### 4.2.4.9 THE 1992 MASTERPLAN - STREETSCAPE STRATEGY

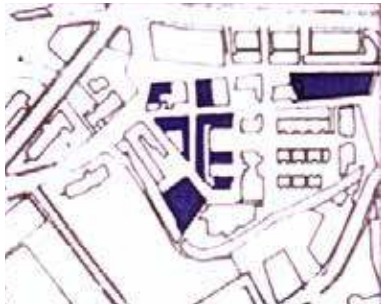
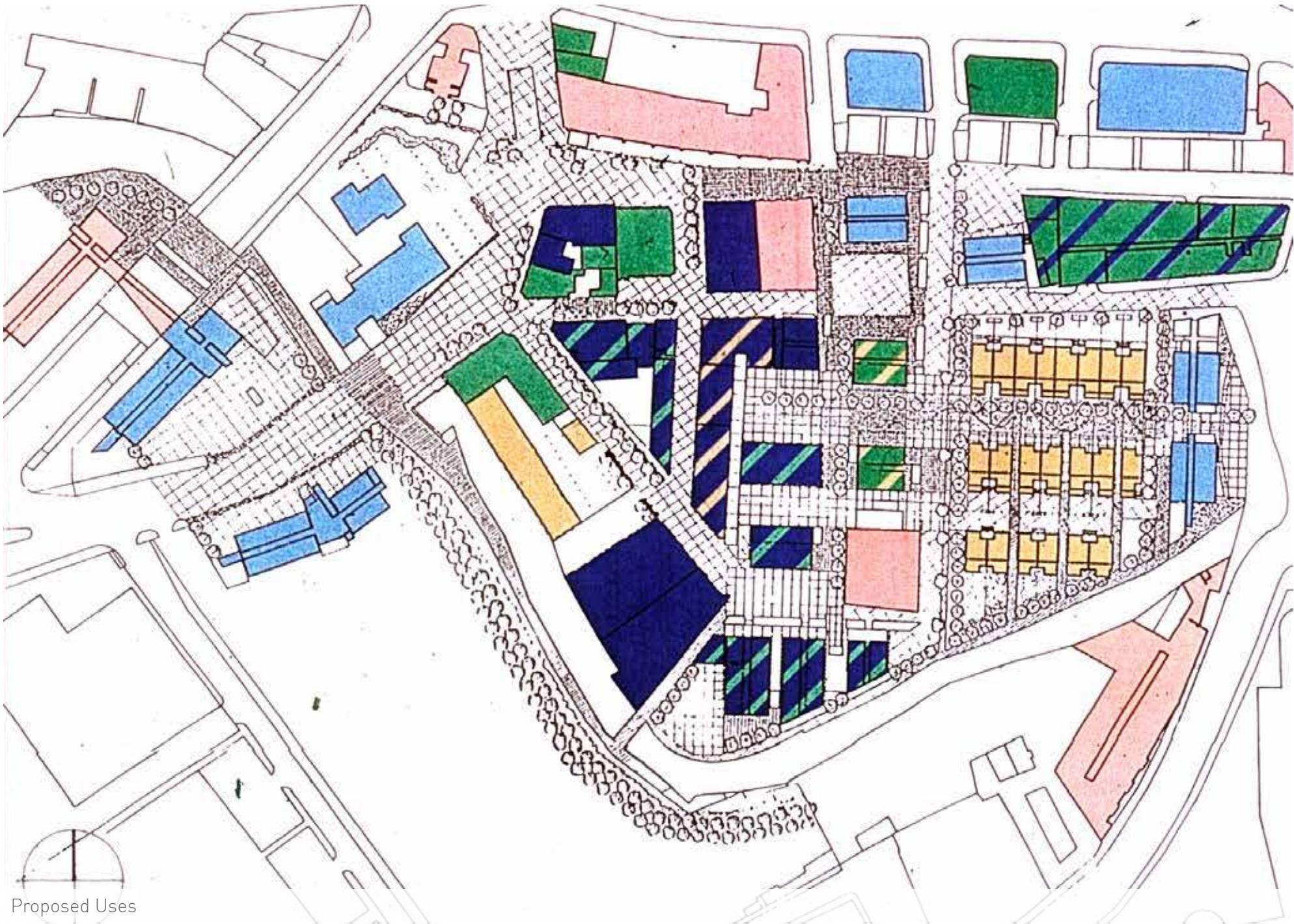
The streetscape strategy aimed to give identity to functional routes for both vehicles and pedestrians within Knott Mill. In this way, it was considered the existing urban character of Knott Mill should be respected.

The main aims of the streetscape proposals were to:

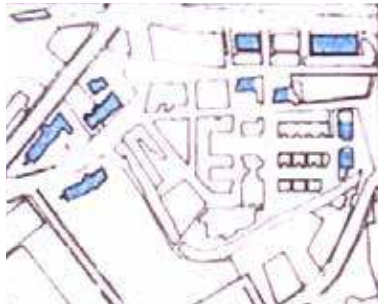
- Identify and enhance the pedestrian realm;
- Clarify vehicular movement, particularly in relation to pedestrian traffic;
- Upgrade the perception of Knott Mill using quality materials and detailing.

The streetscape proposals sought to implement these aims through the careful integration and transition of materials.

- The roads were generally surfaced with 'Tegular' setts and footpaths clearly defined with high quality paving. Some secondary streets of the core area were to be kerbless, with the aim of reducing the dominance of vehicles.
- Pedestrian priority was to be established on routes within Knott Mill by revamping streets and introducing traffic-calming measures.



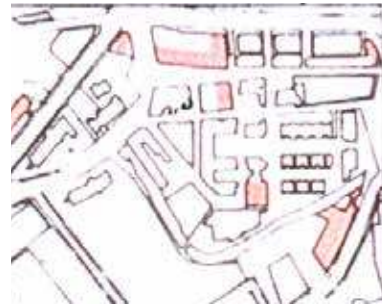
Studios/Offices



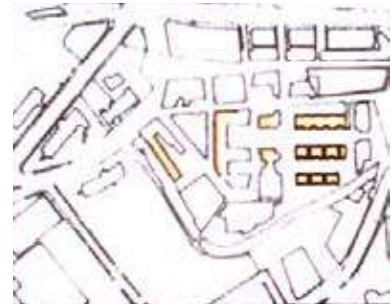
Office Commercial



Retail/Workshop

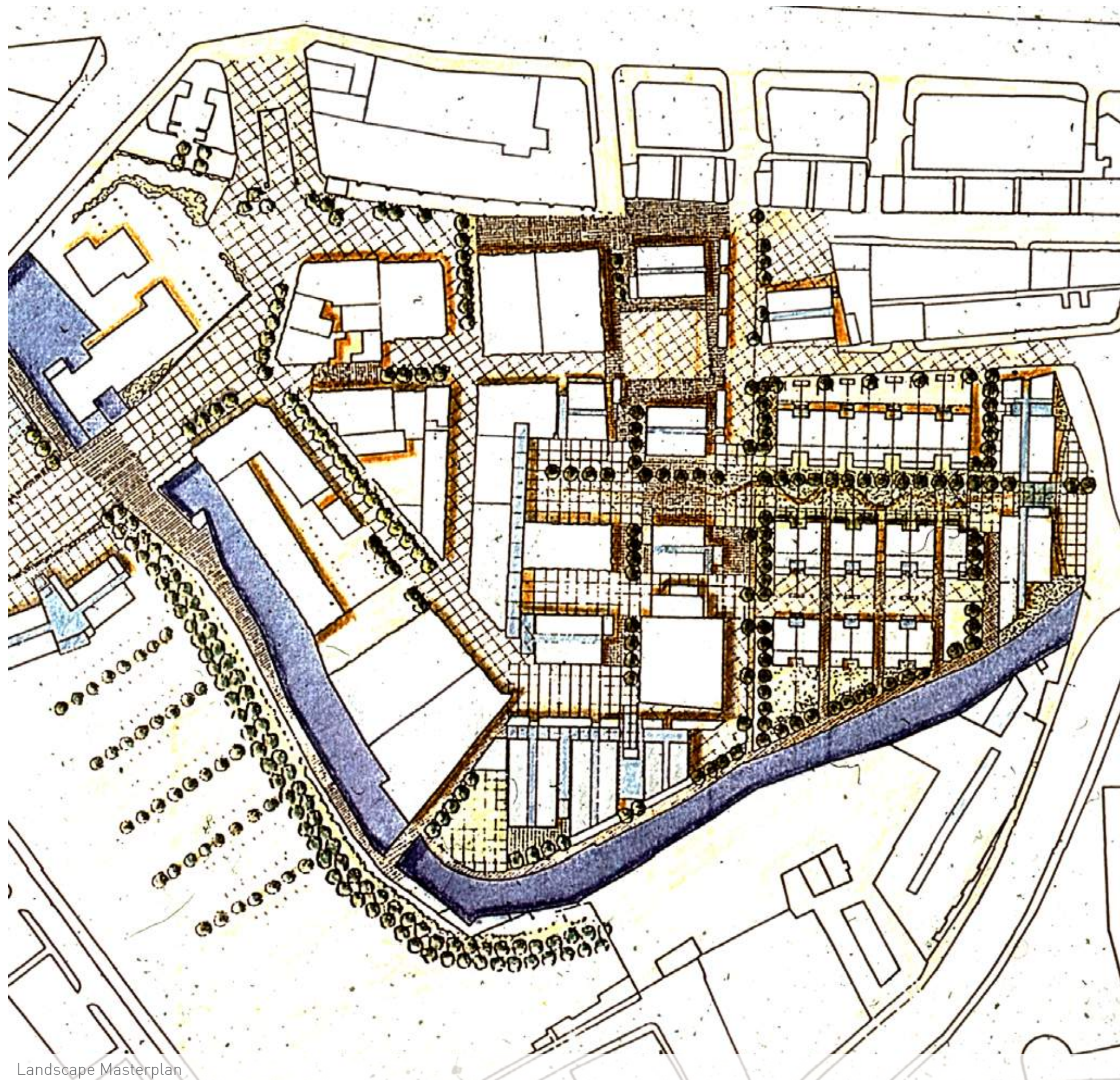


Leisure/Bars/Restaurants



Residential





Landscape Masterplan

## 4.2 Site History

### 4.2.4.10 THE 1992 MASTERPLAN - LANDSCAPE STRATEGY

The landscape strategy formed part of the coordinated improvements to the surrounding area and the links to surrounding districts. It was considered that the external spaces would determine the accessibility and liveliness of the area and contribute greatly to its revival.

The masterplan's landscape approach was considered to respect the strong urban character of Knott Mill, with the components of paving and greenery used simply and sparingly to highlight areas of movement and meeting.

The landscape spaces were to be characterised by:

- Landmark office buildings along Deansgate and City Road in which landscape would unify and control the disjointed townscape;
- Residential buildings where provision would be made for gardens and domestic scale space; and
- Attractive leisure uses associated with Deansgate where the townscape would provide high quality surfaces and finishes.

### 4.2.4.11 THE 1992 MASTERPLAN - APPRAISAL

Save for the environmental improvements works, most of the key strategies of the 1992 Development Strategy were not implemented. However, many of the key strategies are still relevant to the current aspirations for Knott Mill, as set out in this Masterplan. The clear aspiration to improve connections, both within Knott Mill and also its linkages with surrounding areas is evident. The aspiration for a mixed-use area bringing vibrancy is still an important aim. In addition, the importance of an improved pedestrian environment throughout Knott Mill and in particular at the Deansgate Rail Station interface is as pertinent today as it was in 1992.

However, a critical difference between 1992 and today is that Knott Mill sits in an entirely different contextual setting. In 1992, Knott Mill was positioned in an edge-of-city location, whereas now, it occupies a more central city centre location in a city whose notional boundary is shifting ever southwards.



## 4.0 STUDY AREA APPRAISAL

### 4.2 Site History

#### 4.2.5 BUILDINGS OF CULTURAL VALUE OVERVIEW

Knott Mill played host to a special time in Manchester’s evolution as the home of a number of key figures of the 1980s and 1990s musical and cultural scene. Several buildings of cultural value can be identified within the Study Area as detailed below. Given their cultural value and their role in the formation of Knott Mill’s unique identity, these buildings have been retained as part of the Masterplan strategy.

#### 4.2.6 THE BOARDWALK AND TJ DAVIDSON’S REHEARSAL AND RECORDING STUDIO (01)

The Boardwalk nightclub on Little Peter Street was a popular live music venue in the late 1980s and early 1990s and was owned by Colin Sinclair. The Stone Roses, Charlatans, Happy Mondays and James played the opening night in 1986, with bands such as Oasis and Northside making their live debuts at the club, demonstrating its importance as a live venue for local bands. The promo video for Joy Division’s “Love Will Tear Us Apart” was also filmed at the venue.

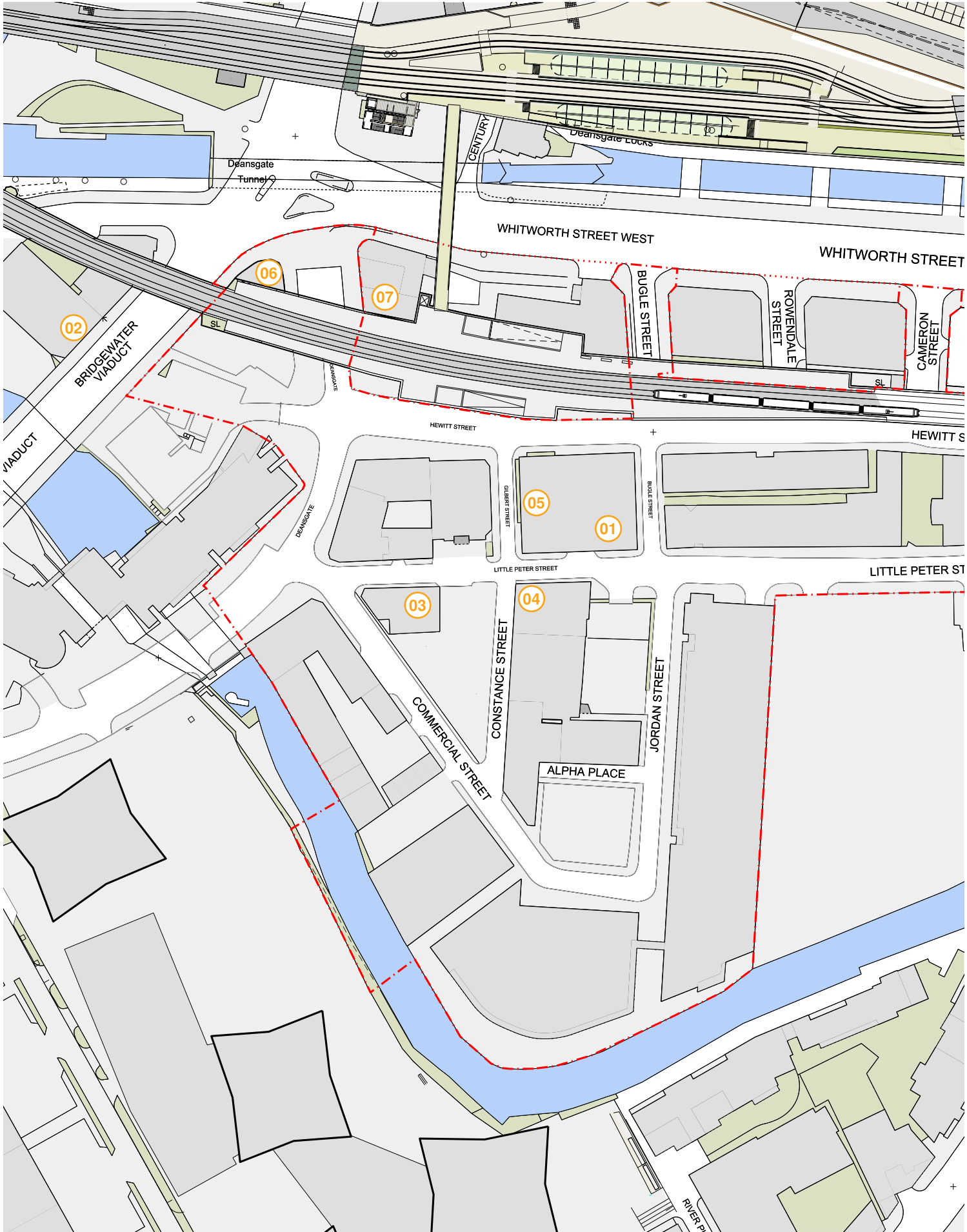
In later years and until the club closed in 1999, former Hacienda DJ, Dave Haslam, played the regular and popular ‘Yellow’ night. Funkademia was started by DJ and promoter, David Payne, at The Boardwalk in 1995 and went on to become Manchester’s longest running club night.

#### 4.2.7 PETE WATERMAN’S RECORDING STUDIO (02)

Outside of the Study Area, the former Castlefield Congregational Chapel, built in 1858, was designed by local architect Edward Walters and designated by English Heritage as Grade II listed. The building was converted to a sound recording studio in the 1980s and owned by Pete Waterman, part of the influential, Stock Aitken and Waterman songwriters and record producers. Rick Astley recorded “Never Going to Give You Up” there. Waterman went on to sell the building in 2006 and it has since been converted to offices.

#### 4.2.8 PLANET FOUR RECORDING STUDIO (03)

Planet4 is a Manchester landmark where artist, Chris Joyce of Simply Red, recorded. Planet4 served as recording studios and more recently has been converted into offices following the grant of planning permission in 2003. The premises are now occupied by property developers Capital and Centric.





### 4.2 Site History

#### 4.2.9 TONY WILSON'S LOFT APARTMENT (04)

At the top of a brick stairwell is a vast, open loft space which was once a Victorian pressing plant. It was converted into a high-spec apartment by designer Ben Kelly, with Tony Wilson, of Factory Records fame, taking residence.

The design is stylish, yet uncompromising and has been described as embracing Manchester's industrial heritage and giving the building a future. With minimal interference, the room is vast and simply has two bedrooms and a bathroom partitioned off to one side and a basic kitchen in one corner. This space has preserved and highlighted some of the buildings original features, including ornate cast metal columns, exposed brickwork and wooden beams.

#### 4.2.10 ROB GRETTON'S OFFICE (05)

On the corner of Gilbert Street and Hewitt Street lies the former Factory Records office owned by Rob Gretton. The Manchester-based, British, independent record label was originally established on Palatine Road in Didsbury but later relocated to Knott Mill in 1990 and was responsible for musical acts including Joy Division, New Order, the Happy Mondays, and James.

#### 4.2.11 ATLAS BAR (06)

Between the railway arches of Deansgate, Ian Simpson, Rachel Haugh, Nick Johnson and Jenny Thompson converted a car repair shop into a quirky but elegant bar and café using simple materials such as plywood cladding to the arches and reclaimed floorboards. Atlas Bar opened in 1993 and was one of Manchester's first independent and most successful bars.

It is spacious with a modern, chic feel, accentuated by simple metal tables and minimalist seating. Throughout the last decade Atlas Bar became a popular meeting point for local creatives and hub of social activity.

#### 4.2.12 ATLAS DELICATESSEN (07)

The Atlas Delicatessen neighboured Atlas Bar, and was developed following the growing success of Atlas Bar. Again, developed by Ian Simpson, Rachel Haugh, Nick Johnson and Jenny Thompson, 'the deli' (as it was known locally) opened and served quality food and drink. Ownership was subsequently passed to Chris Joyce of Simply Red fame and the deli was re branded as Love Saves the Day. The deli ceased trading in 2005 and the unit is now operated as a local convenience store.



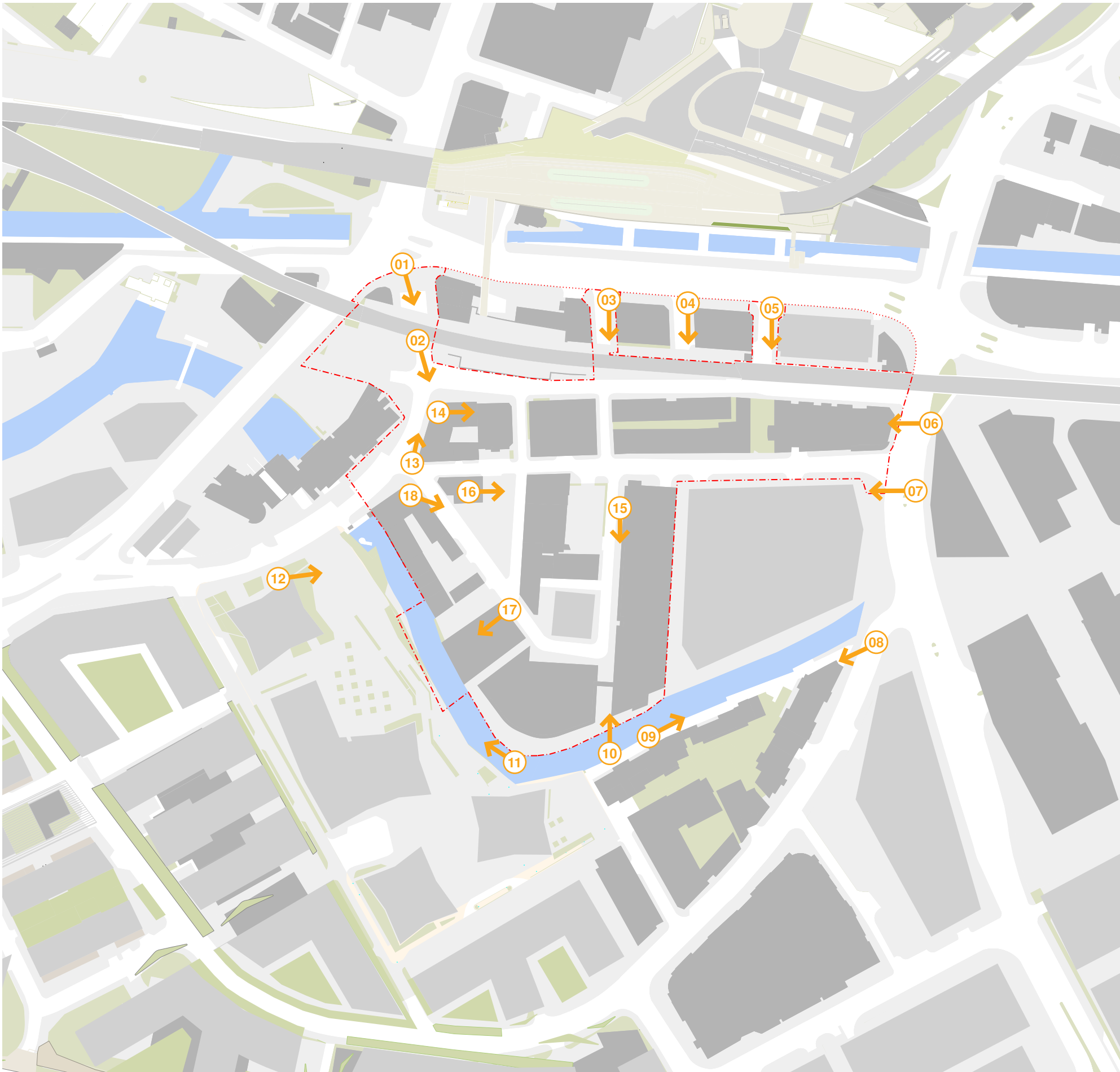


4.0 STUDY AREA APPRAISAL

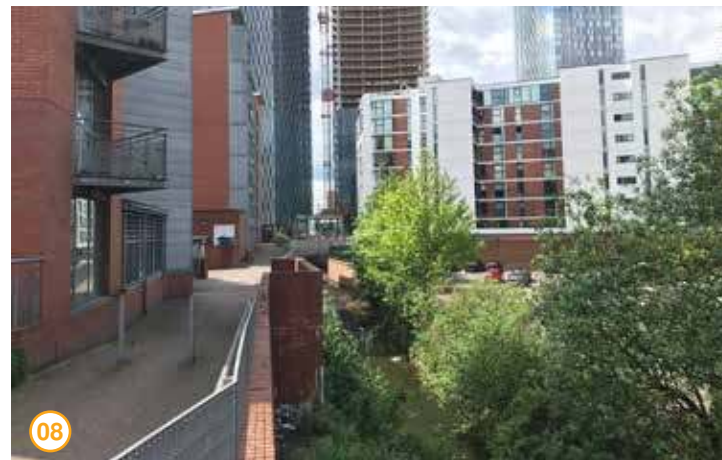
4.3 Present Views and Vistas

4.3.1 OVERVIEW

The photographs adjacent are taken in the present day from key locations within the Study Area. The current construction of the Axis and Deansgate Square towers will alter the setting of some of these views dramatically in the very near future.









# 4.0 STUDY AREA APPRAISAL

## 4.4 Existing Land Uses

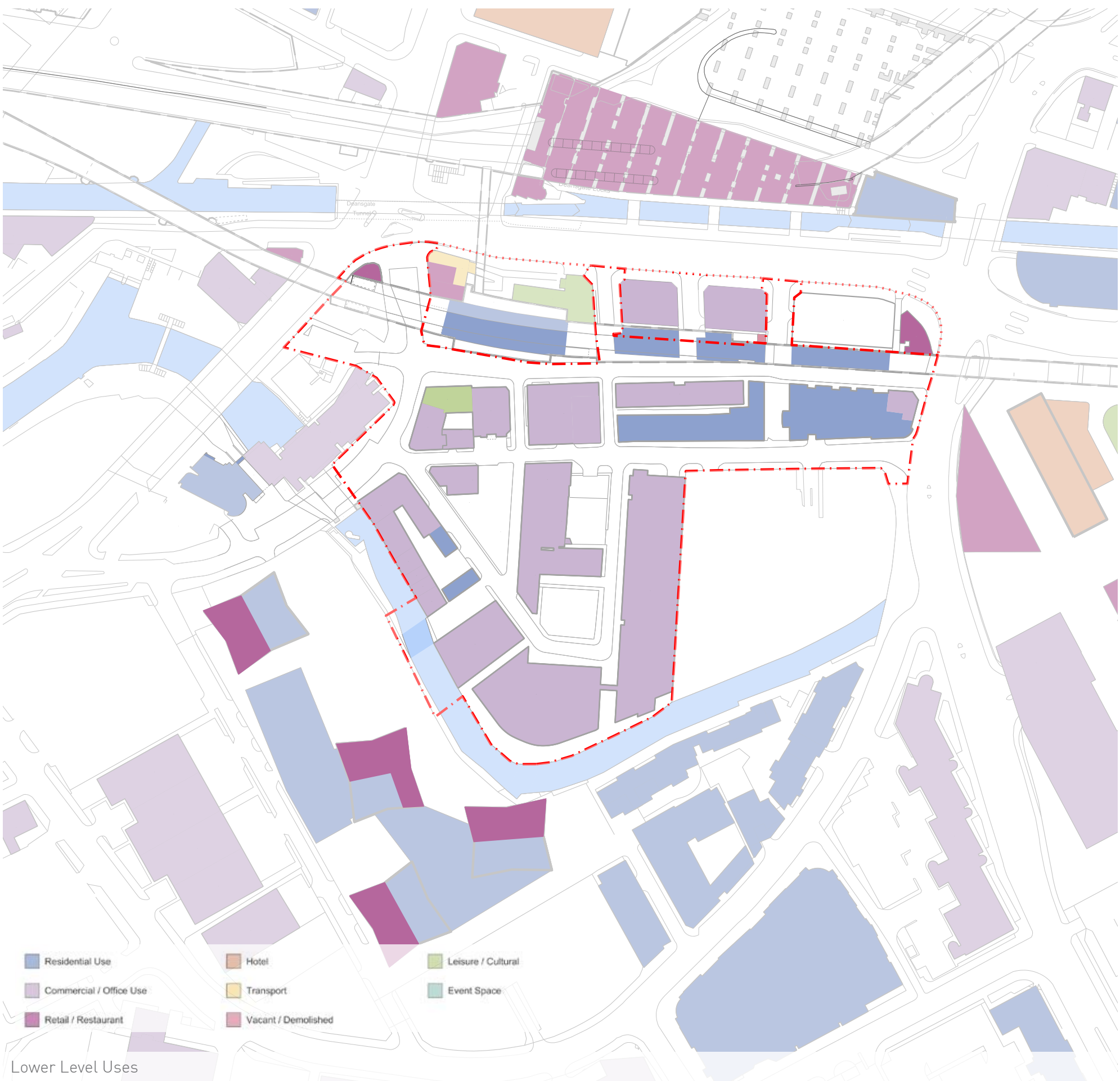
### 4.4.1 OVERVIEW

Whilst the urban realm in Knott Mill is generally good, surface car parks, under-utilised buildings and gap sites tend to have a negative impact on the local townscape. The quality of the public amenity space and public realm are also lacking.

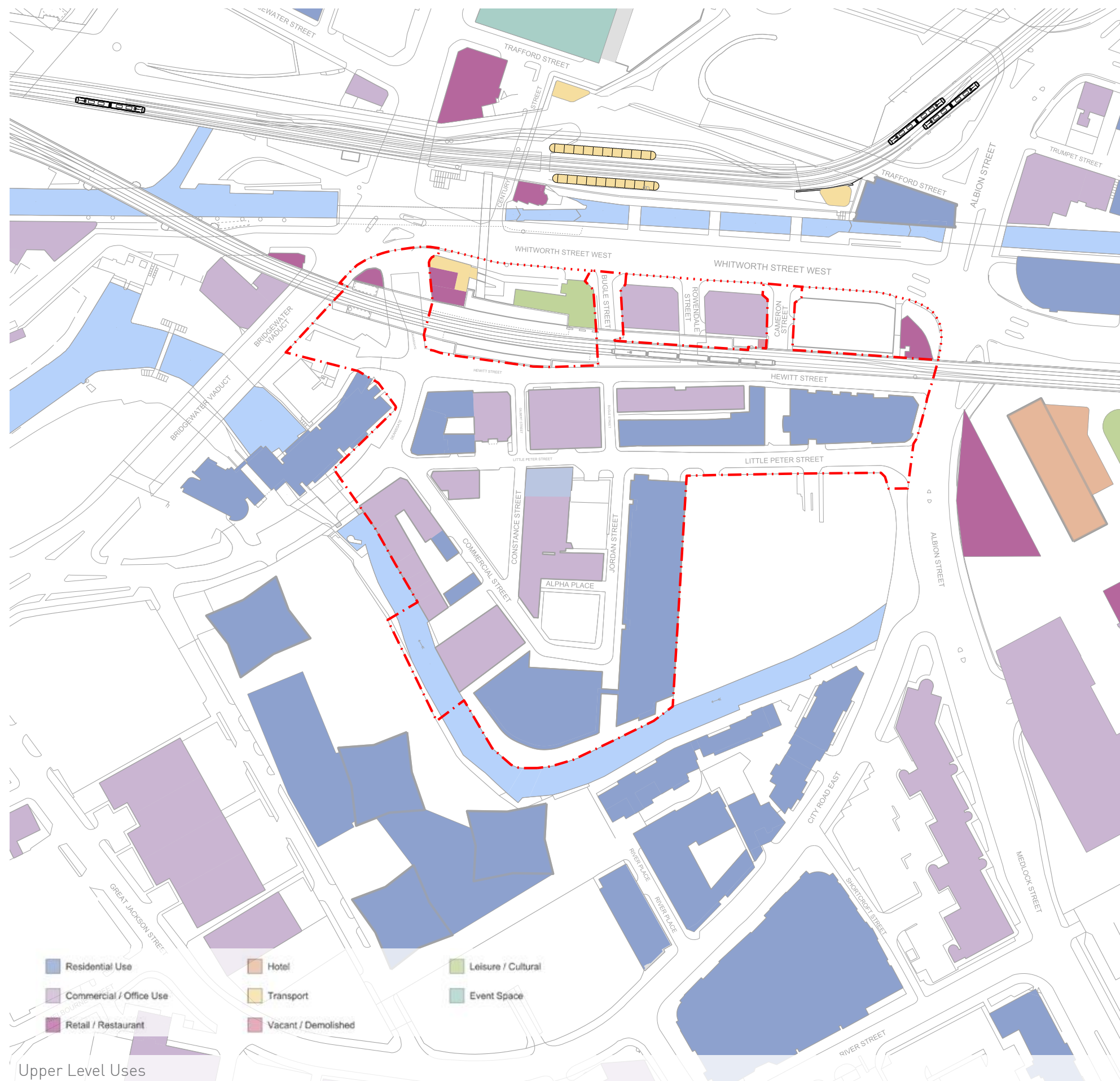
The Study Area is characterised by a mixture of uses and is distinguished by its diversity of architectural styles and differing scales, albeit the Study Area generally can be classified as a low-to-medium rise area, particularly when compared to its high-rise neighbours.

The surviving warehouses are a distinctive element of the streetscape in the Study Area and reflect its previous industrial character. Many of the warehouses have been refurbished with minimal intervention to adapt to new uses, including; residential apartments, offices, galleries and studios. Commercial and office uses dominate at the lower levels providing an element of ground floor active frontage, whilst a mix of predominantly office and residential uses dominate the upper levels. Small pockets of retail, leisure and cultural uses are also evident.

The diagrams adjacent illustrate the current ground floor and predominant upper floor usage of buildings in the area. Despite some layering and mix of uses, there are opportunities to further animate the area. The construction of the larger scale residential buildings of Hill Quays and Ropeworks have contributed significantly to footfall and activation of streets within the Study Area.







4.0 STUDY AREA APPRAISAL

4.5.1 Land Ownership - Freehold

4.5.1 OVERVIEW

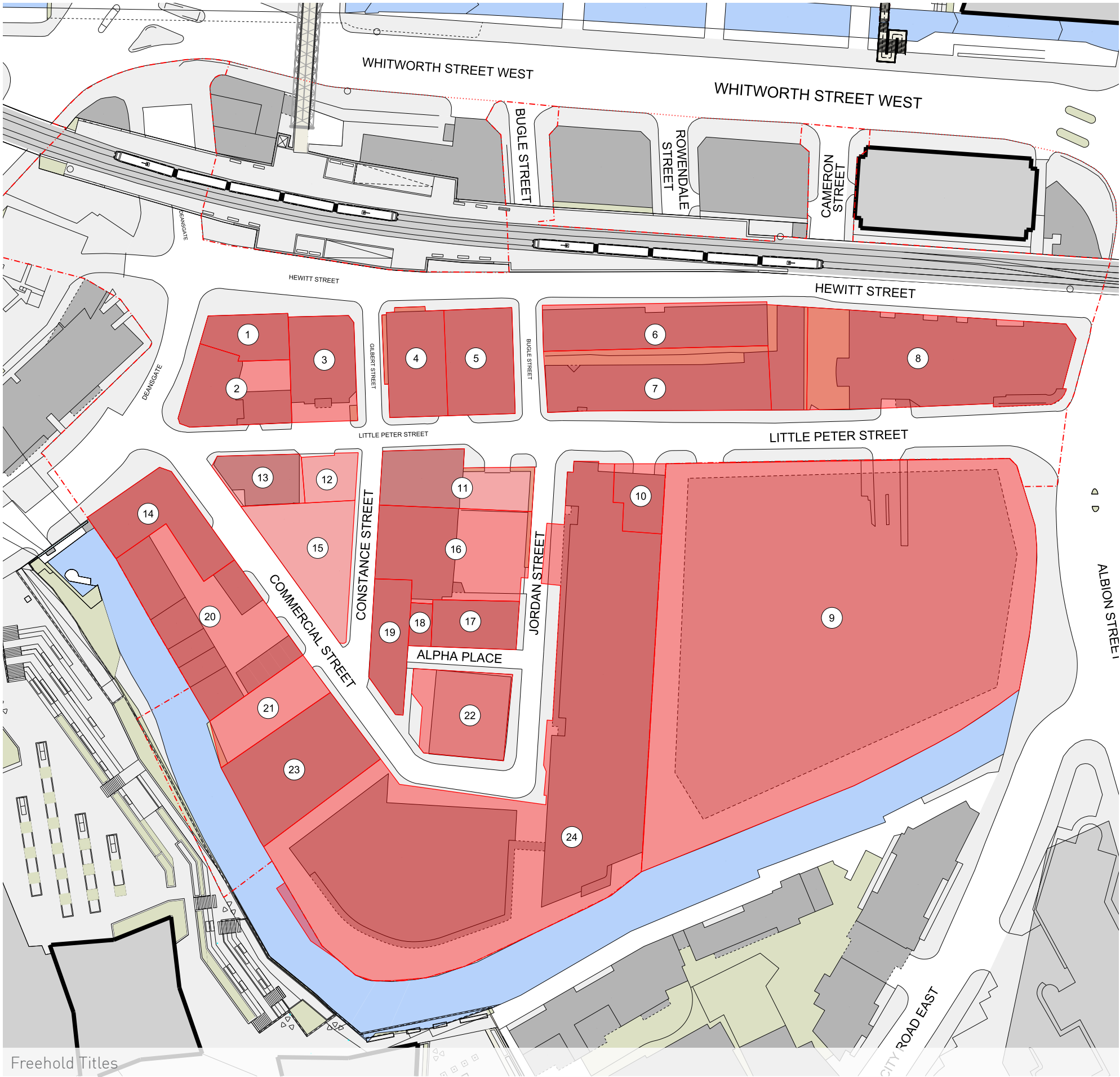
There is a fragmented land ownership pattern with many small plots owned by a variety of predominantly private sector land owners with both freehold and leasehold titles.

There is some potential to combine a few of the smaller plots to create development plots of a reasonable size without impacting on the historic street pattern.

All freehold title owners within the study area have been contacted and invited to contribute to the development of this study as members of the Knott Mill Association. The principal major landowners have influenced the vision for this Masterplan and are supportive of the principles set out in this document.

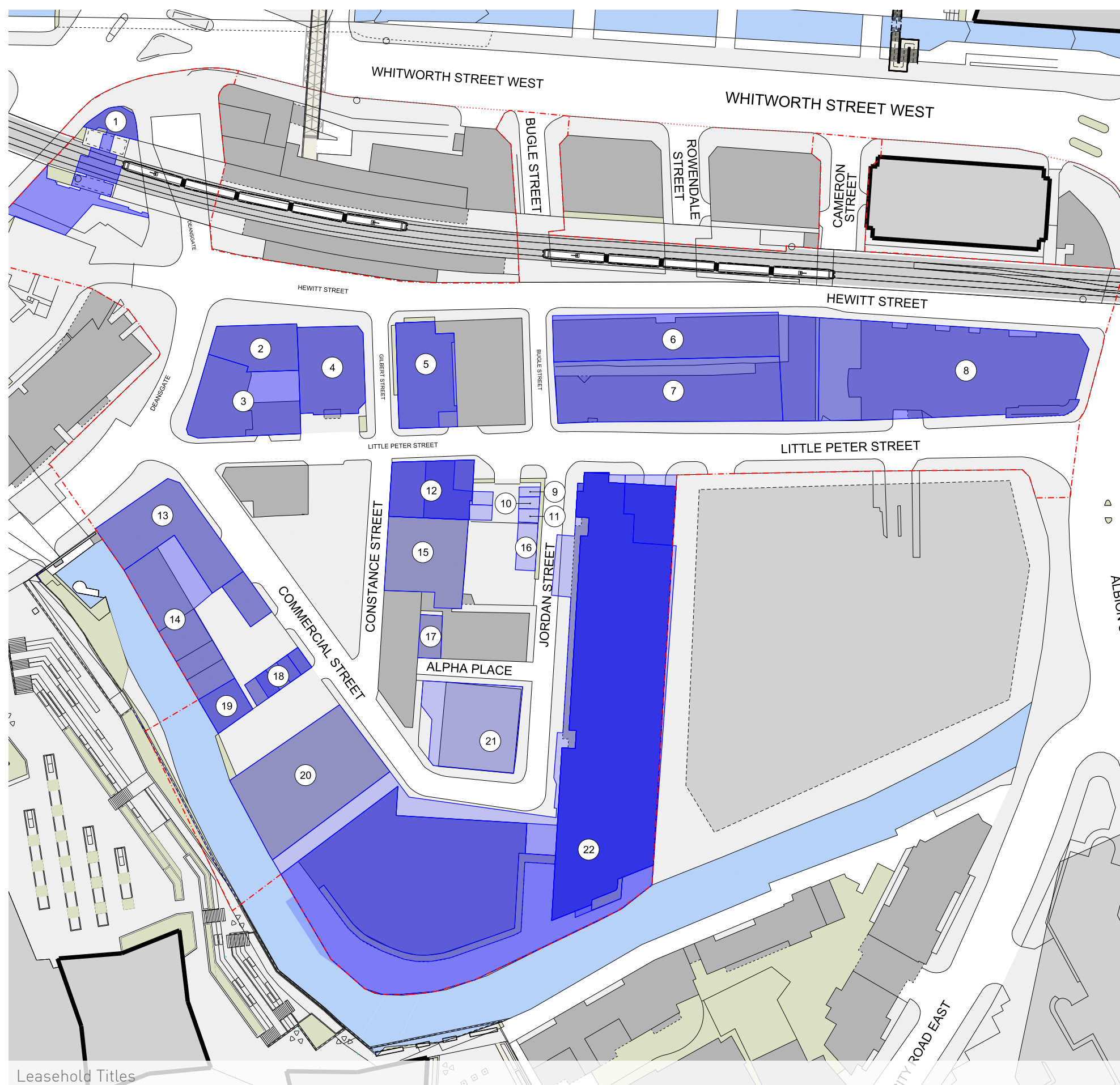
The Masterplan proposals work with the existing ownership boundaries and development principles have been defined to allow delivery of standalone proposals that will each contribute towards the goal of delivering a distinctive and high-quality neighbourhood. Despite the fragmented nature of the Study Area’s land ownership, through the acknowledgement of specific land parcels, it is envisaged developments can progress without the need for lengthy land assembly processes.

1	LA332958	14	GM294205
2	GM852676	15	LA27964
3	LA190724	16	GM858489
4	GM972142	17	GM612454
5	GM432080	18	GM860971
6	GM586471	19	GM488176
7	GM55402	20	LA33160
8	GM612042	21	LA33160
9	LA177747	22	LA33160
10	LA212810	23	LA33160
11	GM736135	24	LA33160
12	LA84450		
13	GM93796		





## 4.5.2 Land Ownership - Leasehold



- 1 GM654493
- 2 MULTIPLE LEASEHOLDS
- 3 MULTIPLE LEASEHOLDS
- 4 MAN218260, MAN213891
- 5 GM721569, MAN31949
- 6 GM983030
- 7 MULTIPLE LEASEHOLDS
- 8 MULTIPLE LEASEHOLDS
- 9 GM786927
- 10 GM786928
- 11 GM739784
- 12 GM742984, GM786927, GM739784, GM786928
- 13 MAN273352, MAN263818
- 14 GM839010, MAN50750
- 15 GM841976
- 16 GM911913
- 17 GM911559
- 18 GM920286, GM937653, GM937653, GM918111, MAN50750, GM839010
- 19 GM920286
- 20 MAN257442
- 21 MAN116204
- 22 MULTIPLE LEASEHOLDS



4.0 STUDY AREA APPRAISAL

4.6 Statutory Designations

4.6.1 OVERVIEW

This section identifies the statutory designations within the Study Area and its immediate surroundings. In addition to several designated heritage assets and an adjacent Conservation Area boundary, several buildings which contribute to the character of Knott Mill have been identified.

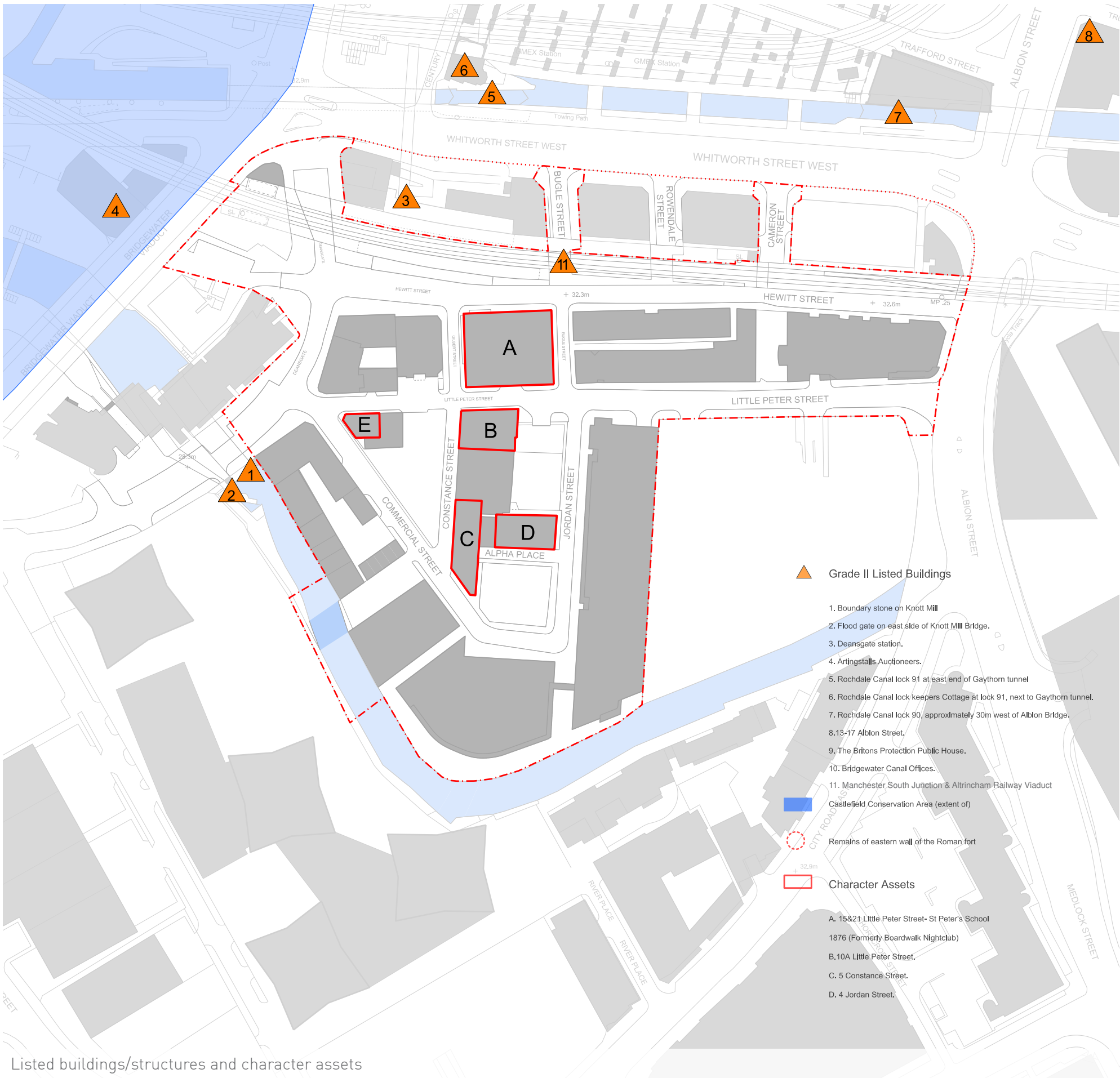
4.6.2 CONSERVATION AREAS

Whilst the Study Area does not fall within a Conservation Area, it sits adjacent to the Castlefield Conservation Area to the west. The Castlefield Conservation Area was designated on 13th October 1979 and its boundary follows the River Irwell, New Quay Street, Quay Street, Lower Byrom Street, Culvercliff Walk, Camp Street, Deansgate, Bridgewater Viaduct, Chester Road, Arundel Street, Ellesmere Street, Egerton Street, Dawson Street and Regent Road. On 26th June 1985, the area was extended by the addition of land bounded by Ellesmere Street, Hulme Hall Road and the River Irwell. It is one of the largest Conservation Areas in Manchester with a focus on the Roman fort from which its name derived - ‘Castle in the Field’.

4.6.3 LISTED BUILDINGS/STRUCTURES

There are no Listed Buildings within the Study Area. However, a discreet Boundary Stone on Knott Mill Bridge carries a grade II listing. In close proximity to the Study Area are also a grade II listed Flood Gate on the east side of Knott Mill Bridge together with Deansgate Station. The ‘Statutory Designations’ diagram demonstrates the location of each of these with a brief description below:

- Boundary Stone on Knott Mill Bridge is a Grade II sandstone asset, dating back to the 18th century. The slab is inscribed with the lettering “township”.
- Flood Gate on the East Side of Knott Mill Bridge is a simple stone structure of national historic importance. The floodgate was built by James Brindley around 1765, and is Grade II listed.
- Deansgate Station is a Grade II listed building, first listed in August 1998. The railway station, dating to 1896, forms the late Victorian replacement to the original 1840s station and holds a curved corner on Whitworth Street West and Deansgate.





## 4.6 Statutory Designations

### 4.6.4 CHARACTER ASSETS

There are five buildings within the Study Area which are also identified on the 'Statutory Designations' diagram as Character Assets. Further information about these buildings is set out below and shown on the corresponding photographs:

A - 15&21 Little Peter Street - St Peters School 1876 (Formerly Boardwalk Nightclub): This building was formally a rehearsal studio and popular live music venue during the late 1980s and early 1990s and was owned by Colin Sinclair. Bands such as Oasis and Northside made their live debuts here, and other bands including the Stone Roses, Happy Mondays, James and Verve appeared frequently. The club closed in 1999 and since then the building has been refurbished for use as offices.

B - 10A Little Peter Street: is an early Victorian warehouse which has been recently renovated into a furniture showroom and residential apartments.

C - 5 Constance Street: is a small Victorian stone built warehouse with its primary façade fronting the narrow, cobbled Constance Street. The building has mostly been converted into offices.

D - 4 Jordan Street: is a four-storey Victorian structure formerly used as a print works. The building was renovated in 2005 and was converted into commercial space.

E - 2 Little Peter Street: is a three storey Victorian brick building, with a triangular footprint which defines the corner of Little Peter Street and Commercial Street. The building was refurbished in 1993, as offices, with an associated recording studio (subsequently converted into offices) contained in an extension on the adjacent plot (4 Little Peter Street).





## 4.0 STUDY AREA APPRAISAL

### 4.7 Planning History

#### 4.7.1 OVERVIEW

A planning history search has been carried out and those applications of particular relevance to the regeneration of Knott Mill are summarised by the 'Planning History' diagram adjacent.

The planning history search reveals several planning permissions have been granted for development at sites and buildings within the Study Area. These either relate to gap sites or to vertical extensions to existing buildings. Whilst some of the permissions have not been implemented, the granting of permission indicates a general acceptance of the principle of a medium-rise height profile (typically four to seven storeys) and a general increase in density in the Knott Mill area.

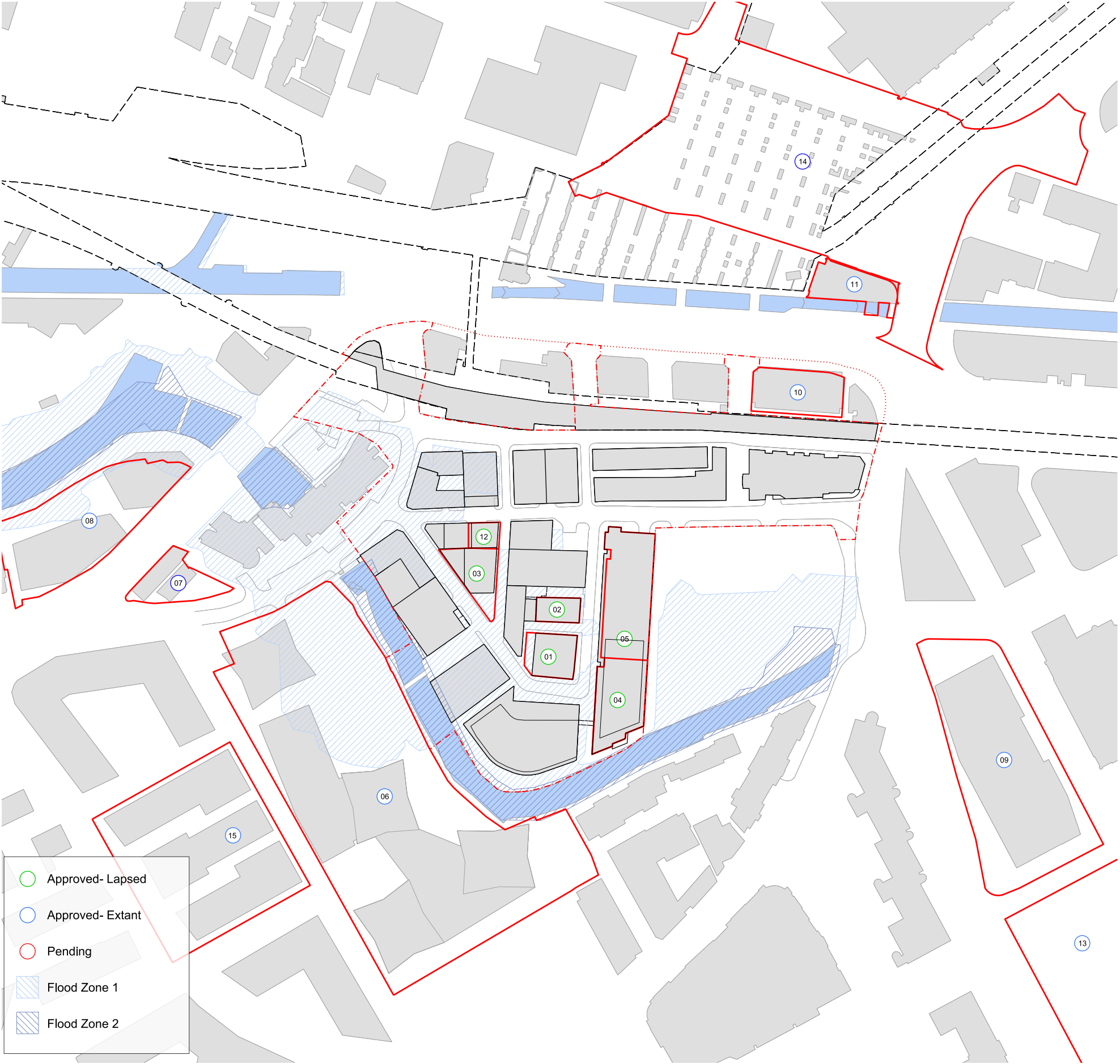
In contrast to this, planning permission has been granted for a range of tall, predominantly residential, buildings in the Study Area's immediate surroundings, many of which have been implemented and are currently being constructed. Permissions for six towers, ranging in height from 27 storeys to 66 storeys have been granted on various sites. This further emphasises the existing lower-rise nature of Knott Mill when compared to its neighbouring areas.

### 4.8 Flood Risk

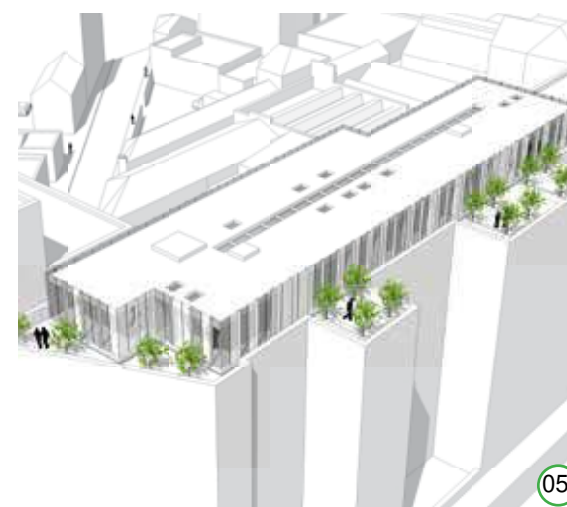
#### 4.8.1 OVERVIEW

According to the Environment Agency's Flood Mapping, parts of the Study Area falls within Flood Zone 2 - land assessed as having a medium probability of flooding. The remainder of the Study Area falls within Flood Zone 1 - land assessed as having a low probability of flooding.

In accordance with the National Planning Policy Framework, a site-specific Flood Risk Assessment is required for new development in Flood Zone 2 as part of any forthcoming planning application.







- 01 108041/FO/2015/C1  
1 Alpha Place, Manchester M15 4PP  
Seven storey of 53 apart-hotel rooms on the upper floors, reception area and bar on the ground floor and restaurant at basement level.  
Approval date: 31.07.2015
- 02 104041/FO/2013/C1  
4 Jordan Street, Manchester M15 4PY  
Change of use of second floor from offices to residential dwelling and alterations to roof to include 9 sky lights.  
Approval date: 19.12.2013  
Planning permission lapsed.
- 03 086164/FO/2008/C1  
1-3 Commercial Street  
Knott Mill, Manchester M15 4PZ  
Five storey office building following demolition of the existing building.  
Approval date: 26.08.2009  
Planning permission lapsed.
- 04 085177/FO/2007/C3  
1 Jordan Street, Manchester  
Erection of 1 penthouse apartment on the rooftop of the existing building.  
Approval date: 10.10.2008  
Planning permission lapsed.
- 05 101994/FO/2013/C1  
Hill Quays, 1 Jordan Street, Manchester  
Erection of a penthouse level to provide 6 new apartments on top of existing building.  
Approval date: 16.07.2013  
Planning permission lapsed.
- 06 111719/FO/2016/C1  
Land Bounded By Owen Street, Pond Street, Deansgate And The River Medlock, Manchester M15 4QA  
Four residential buildings, ancillary amenity building for residents with leisure facilities, ground floor retail, amenity space for residents within residential buildings, new public realm and landscaping, basement car parking and related highway, access, servicing, engineering and associated works.  
Approval date: 04.07.2016
- 07 115591/FO/2017  
Land At Chester Road(A56) Bridgewater Viaduct Manchester M3 4LB  
Thirteen-storey building comprising 53 residential apartments together with ground floor commercial unit, landscaping, loading bay and pedestrian access.  
Approval date: 01.08.2017
- 15 116850/FO/2017  
Plot E, Great Jackson St  
32, 26 and 18 storey buildings containing 399 apartments and 50 retirement living units.  
Approval date: 14.02.2018
- 08 113870/FO/2016  
2 - 4 Chester Road  
Manchester M15 4QG  
Two residential apartment buildings with leisure facilities for residents, new public realm and landscaping including two new pedestrian connections to the Castlefield basin from Chester Road, basement car parking and related highway, access, servicing and associated works.  
Approval date: 05.04.2017
- 09 108963/FO/2015/C1  
Land Bounded By Medlock Street / River Street And Number One First Street, Manchester City Centre Plot 5 Of The First Street Masterplan  
Erection of a 7 storey office building (Use Class B1) including ground floor retail units (use Class A1 (Shop) or A3 (Restaurant and Cafe) or A4 (Drinking Establishment) Uses, public realm and associated works to include brown roofs above 7th floor level.  
Approval date: 31.07.2015
- 10 111003/FO/2016/C1  
10-12 Whitworth Street West  
Manchester M1 5WY  
Demolition of existing building at 10 - 12 Whitworth Street West, and redevelopment of the site for a new 35 storey residential building comprising 327 apartments, with associated residents' services, facilities and communal spaces: servicing, highways, public realm and associated works.  
Approval date: 11.04.2016
- 11 106490/FO/2014/C1  
Land Bounded By Albion Street, Trafford Street & The Rochdale Canal  
Manchester  
Construction of a lower ground plus 27 storey residential building with associated servicing arrangements, hard landscaping, pedestrian access bridge and associated works.  
Approval date: 21.11.2014
- 12 081772/FO/2006/C3  
10 Little Peter Street  
Manchester M15 4PS  
Four storey development comprising of retail unit at ground floor and upper floor offices following demolition of the existing building.  
Approval date: 23.04.2007  
Planning permission lapsed.
- 13 111240/FO/2016/C1  
Plot 6 & 7 First Street  
Manchester M15 4FN  
Creation of a temporary landscaped area in advance of permanent redevelopment.  
Approval date: 19.04.2016
- 14 115871/LO/2017  
Former Bauer Millet site. 40 storey residential tower and 31,000 sq m office building.  
Approval date: 27.07.2017



4.0 STUDY AREA APPRAISAL

4.9 Transport and Movement

4.9.1 EXISTING VEHICULAR MOVEMENT PATTERNS

The Study Area is situated immediately to the south of Whitworth Street West (B6469), to the east of (old) Deansgate (turning into Chester Road) (A56), and to the west of Albion Street (turning into Medlock Street) (A5103): the latter two of which are main arterial routes into and out of Manchester city centre. The Study Area is within easy access of the city’s Inner Ring Road (Mancunian Way (A57)) which connects with other key arterial routes leading to the strategic motorway network.

The key east to west vehicular movements through the Study Area are provided by Hewitt Street and Little Peter Street, both of which link with the southern extent of (old) Deansgate to the west and Albion Street to the east. Constance Street/Gilbert Street and Jordan Street/Bugle Street provide secondary north to south vehicular movements within the Study Area, with Bugle Street and Rowendale Street providing connections to Whitworth Street West to the north. Commercial Street runs in a general south-east to north-west direction from Jordan Street to Deansgate. The road network within the Study Area is made up of setts which serve to unify the appearance of the street groundscape.

The existing street hierarchy attempts to minimise opportunities for ‘rat-running’.

4.9.2 EXISTING PEDESTRIAN MOVEMENT PATTERNS

The major pedestrian and cycle routes in the vicinity of Knott Mill are concentrated at the main arterial routes surrounding the Study Area including at Deansgate, Albion Street and Whitworth Street West, where dedicated cycle infrastructure and pedestrian crossing facilities are provided at main signal-controlled junctions. A Mobike parking zone has recently been introduced at the Deansgate/Whitworth Street junction. Footways are present on most of the roads surrounding the Study Area. There is also a pedestrian refuge on Albion Street (A5103) just south of its junction with Little Peter Street which facilitates pedestrian movement. To the north of the Study Area are signal-controlled pedestrian crossings on all arms of the Albion Street (A5103)/Whitworth Street (B6469) signalised junctions. To the south of the Study Area are signal-controlled pedestrian crossings on all arms of the Chester Road (A56) interchange with the Mancunian Way (A57). Whilst there are pedestrian facilities in the vicinity of the Study Area, the pedestrian environment along the major roads is currently poor.

Ease of pedestrian movement within the Study Area is reasonably good, particularly when compared with the restrictions which limit Knott Mill’s vehicular permeability. However, whilst the street arrangement within the Study Area promotes west to east and north to south pedestrian routes, the physical and psychological effect of the River Medlock as a barrier to pedestrian movements results in pedestrians being directed towards the northern area of the Study Area and to the Whitworth Street West, Albion Street, and Deansgate linkages beyond, with little permeability to Knott Mill’s neighbours to the south or along the southern boundary. Therefore, there is

significant scope to improve pedestrian and cycle connectivity to the south and east of Knott Mill and to its neighbours.

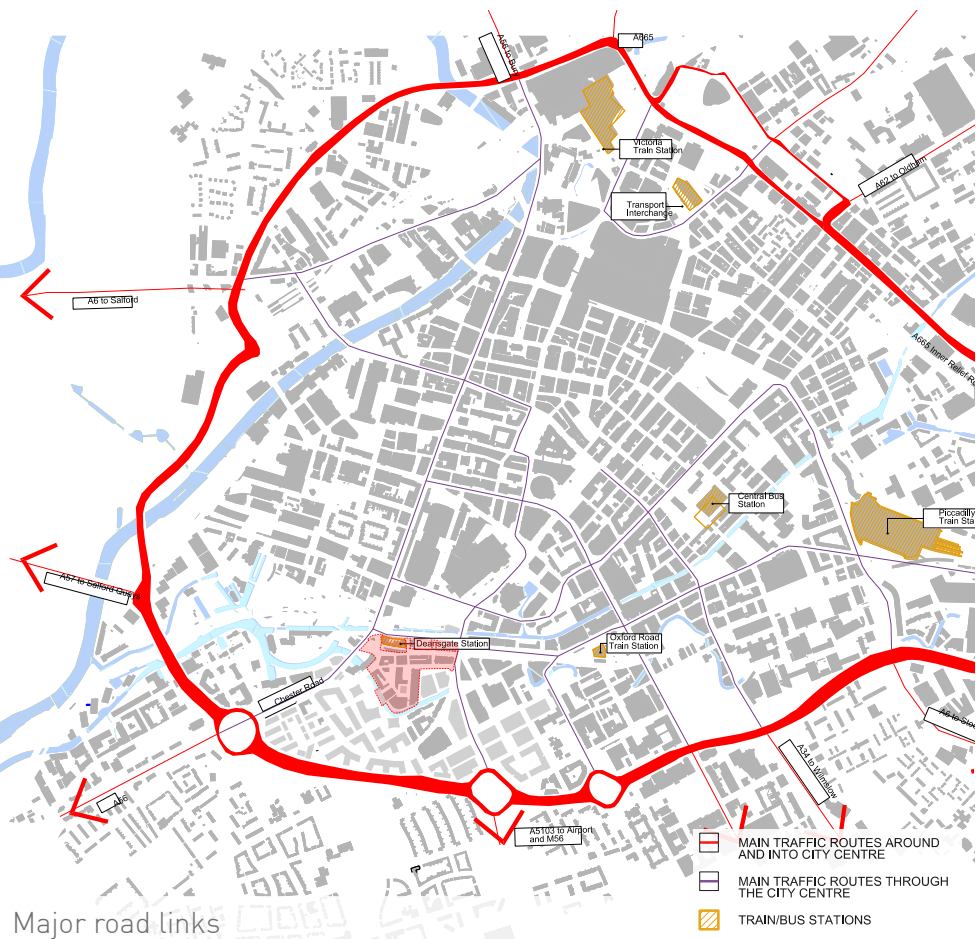
Pedestrian and cycle connectivity to the city centre could also be improved. In particular, the Deansgate Station railway viaduct acts as a physical and psychological barrier to pedestrian movement between the city centre and Knott Mill. The quality of the routes through the viaduct to Hewitt Street in terms of pedestrian experience is poor. In addition, the pedestrian environment along Hewitt Street, Bugle Street, Rowendale Street and Cameron Street could be improved by making this area more attractive and easily navigable by pedestrians and cyclists.

The anticipated increase in pedestrian movements to, from and through the Study Area, as a result of the regeneration of the area will necessitate improvements to pedestrian infrastructure. There is also little penetration of cycle routes into the Study Area, with these predominantly located at the surrounding main arterial routes.

There is a clear opportunity to build upon the city’s current investment in cycle infrastructure and the existing facilities within the vicinity of the Study Area to enhance cycle linkages from, through, and around the city centre as part of sensitively designed pedestrian and cycle access strategy.

4.9.3 CAR PARKING ANALYSIS

The Study Area accommodates several surface car parks which are generally in private ownership, together with a number of on-street pay and display car park spaces. In addition, the surrounding area also provides several facilities for both long and short stay parking either at surface car parks, multi-storey car parks or at permitted on-street locations. There is a large public surface car park at Little Peter Street which interfaces with the Study Area (and sits within the First Street SRF). In addition, further local car parking provision includes the Q-Park multi-storey car park at First Street, the NCP Deansgate car park, and the NCP Manchester Central car park.





## 4.10 Relationship to Public Transport

### 4.10.1 OVERVIEW

The Study Area is highly accessible, in close proximity to, and accessible by, many forms of sustainable transport. The masterplan seeks to promote all opportunities to build on this and to maximise sustainable and active travel options and reduce car usage.

### 4.10.2 MANCHESTER AIRPORT

Within the UK, Manchester Airport offers the largest network of destinations served outside of London. Manchester Airport offers direct daily services to many European and long-haul destinations that rival many available from some European capital cities.

Manchester Airport is easily accessible by Metrolink from Deansgate Castlefield tram stop and by rail from Deansgate and Oxford Rd stations.

### 4.10.3 RAIL

The Study Area is located immediately adjacent to Deansgate Rail Station, a major regional interchange serving destinations to the north and west of Manchester, including direct services to Liverpool and Preston.

The station is also served by trains to Manchester Piccadilly, with a journey time of four minutes. Manchester Piccadilly Station is the main rail hub for the North West, with frequent and rapid services throughout the day and into the evening between Manchester and London with a journey time of only two hours and ten minutes.

### 4.10.4 METROLINK

Metrolink transports on average seven million passengers a year to and from Manchester City centre and has been the subject of significant investment and expansion of routes in recent years. The Metrolink runs to Ashton-under-Lyne, Altrincham, St Werburgh's Road, East Didsbury, Eccles, MediaCity:UK, Droylsden, Oldham, Rochdale, Bury and Manchester Airport.

Deansgate-Castlefield Metrolink Stop is within a minute's walk of the Study Area and is a popular transport option for commuters, shoppers and tourists. Routes generally operate until midnight during weekdays and until 1am at weekends.

### 4.10.5 BUS

Whitworth Street West, Albion Street/Medlock Street and Deansgate/Chester Road all sit within major bus corridors, accommodating an extensive range of bus services. Several bus stops are located within easy walking distance of the Study Area along these main arterial routes.

The £122 million Bus Priority Package now enables city centre bus services to run directly through the heart of the city centre- free from other traffic.

### 4.10.6 FREE BUS SERVICE

The free city centre bus service provides a regular and efficient service connecting the major transport hubs of Piccadilly, Victoria and Shudehill Interchange, as well as the major shopping, cultural, heritage and business quarters.

### 4.10.7 ACTIVE TRAVEL OPTIONS

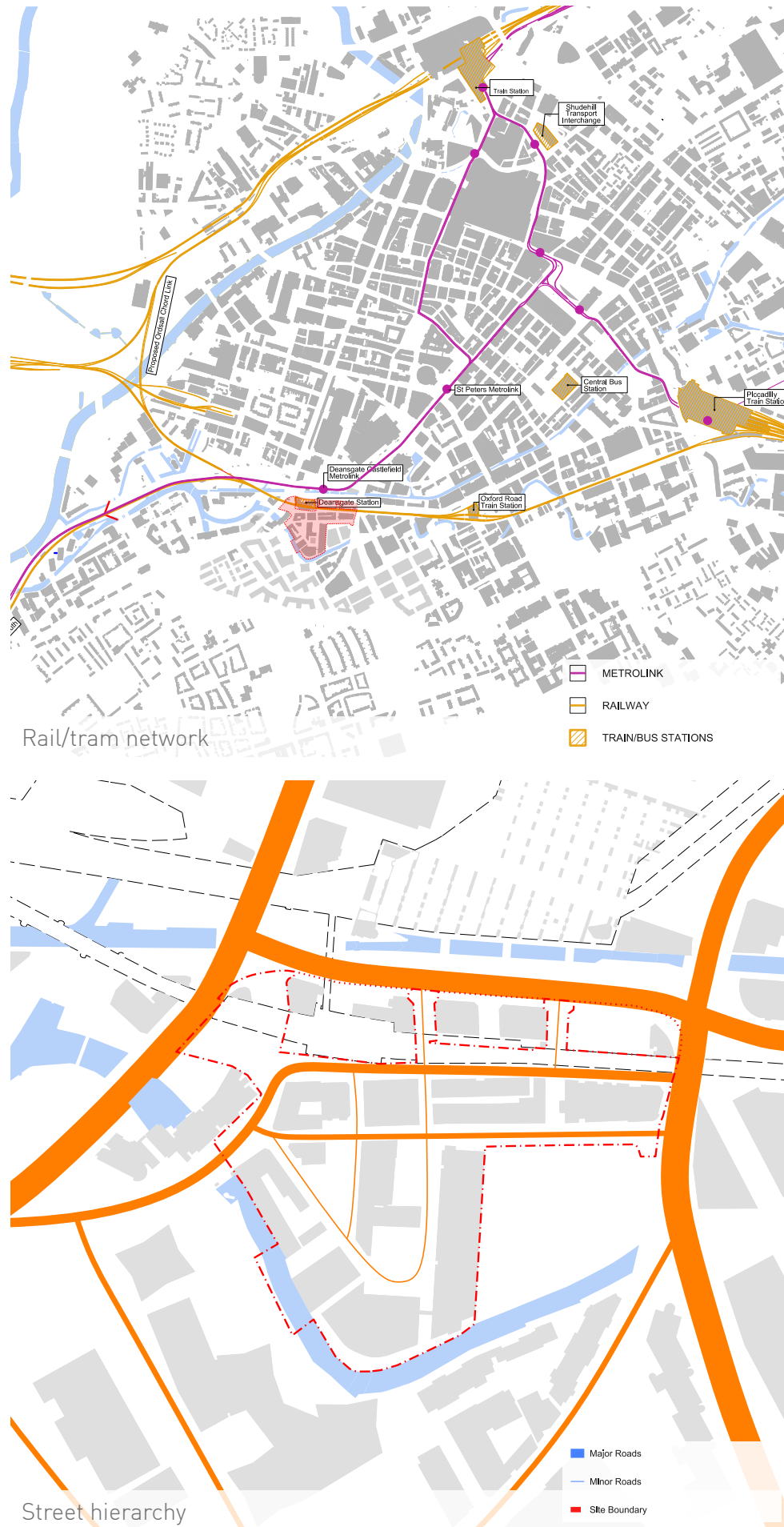
The masterplan seeks to promote active travel options. Through the planning process, developers will be expected to provide facilities for cyclists such as cycle parking, showers, changing and storage.

The masterplan proposals include a pedestrian bridge over the River Medlock and the potential for this to be available for cyclists also is supported and should be explored as the detailed design progresses.

### 4.10.8 REDUCING RELIANCE ON PETROL DRIVEN PRIVATE CARS

As set out above, Knott Mill's highly accessible location and proximity to a range of sustainable modes of transport should be a central consideration in forming and justifying car parking strategies for any future development in the area.

The masterplan will seek to promote alternatives to the use of petrol driven private cars, such as providing locations for car clubs and incorporating charging points for electric vehicles into any future car parking provision.





4.0 STUDY AREA APPRAISAL

4.11 Existing Townscape Analysis

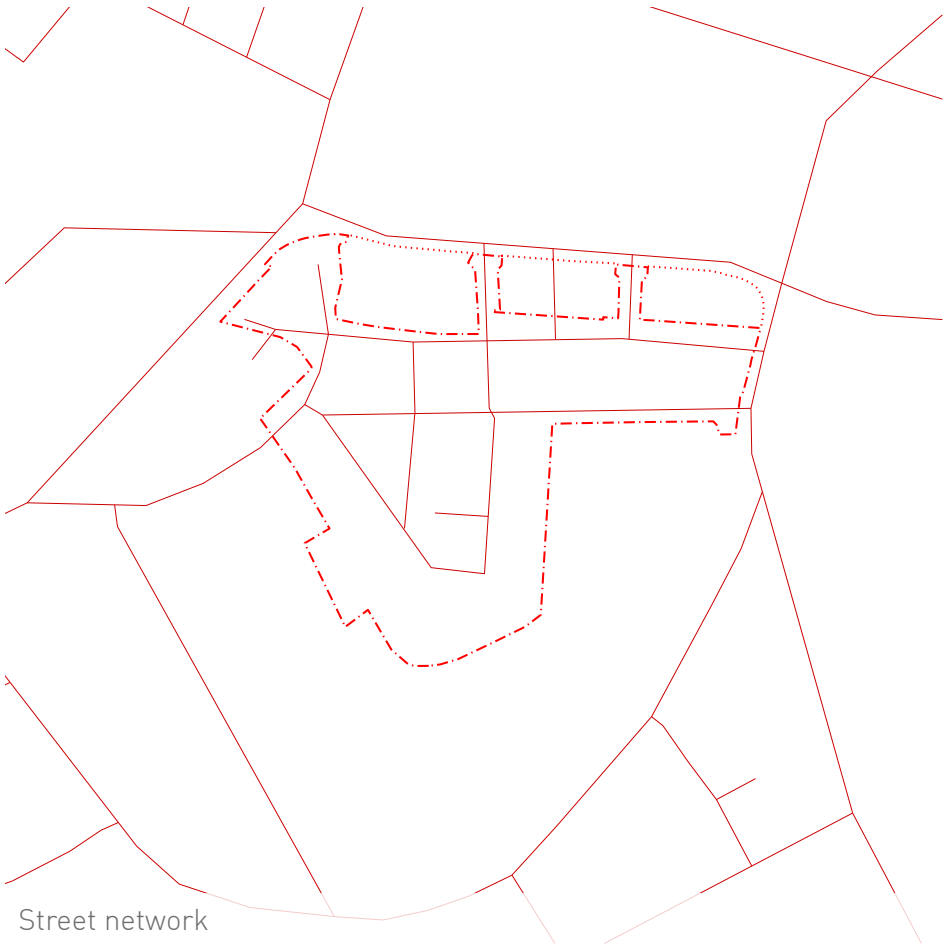
4.11.1 LINKAGES AND CONNECTIVITY

A number of key pedestrian connections link Knott Mill northwards towards the rest of the city centre. The main routes into and out of the Study Area are found at Deansgate and Albion Street via Hewitt Street, Little Peter Street, and Commercial Street. Secondary routes, via Bugle Street and Rowendale Street provide access to Whitworth Street West and the City’s core beyond.

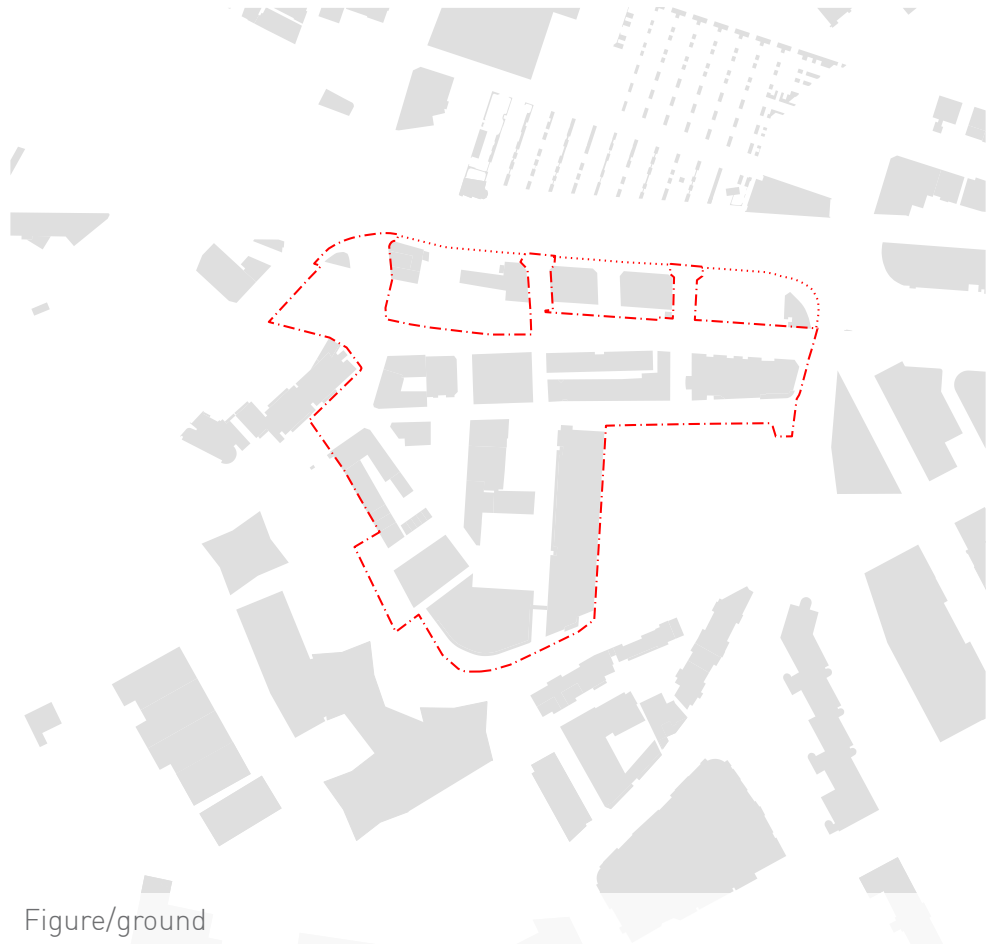
Connectivity to neighbouring areas to the south of the Study Area is also limited due to the presence of the River Medlock. This physical and psychological barrier also results in limited pedestrian activity in the southern part of the Study Area. Significant scope to improve upon the current situation exists. Enhancements would enable the reinforcement of desire lines and make for improved pedestrian permeability of the Study Area and connectivity to neighbouring areas.

4.11.2 URBAN CHARACTER AND GRAIN

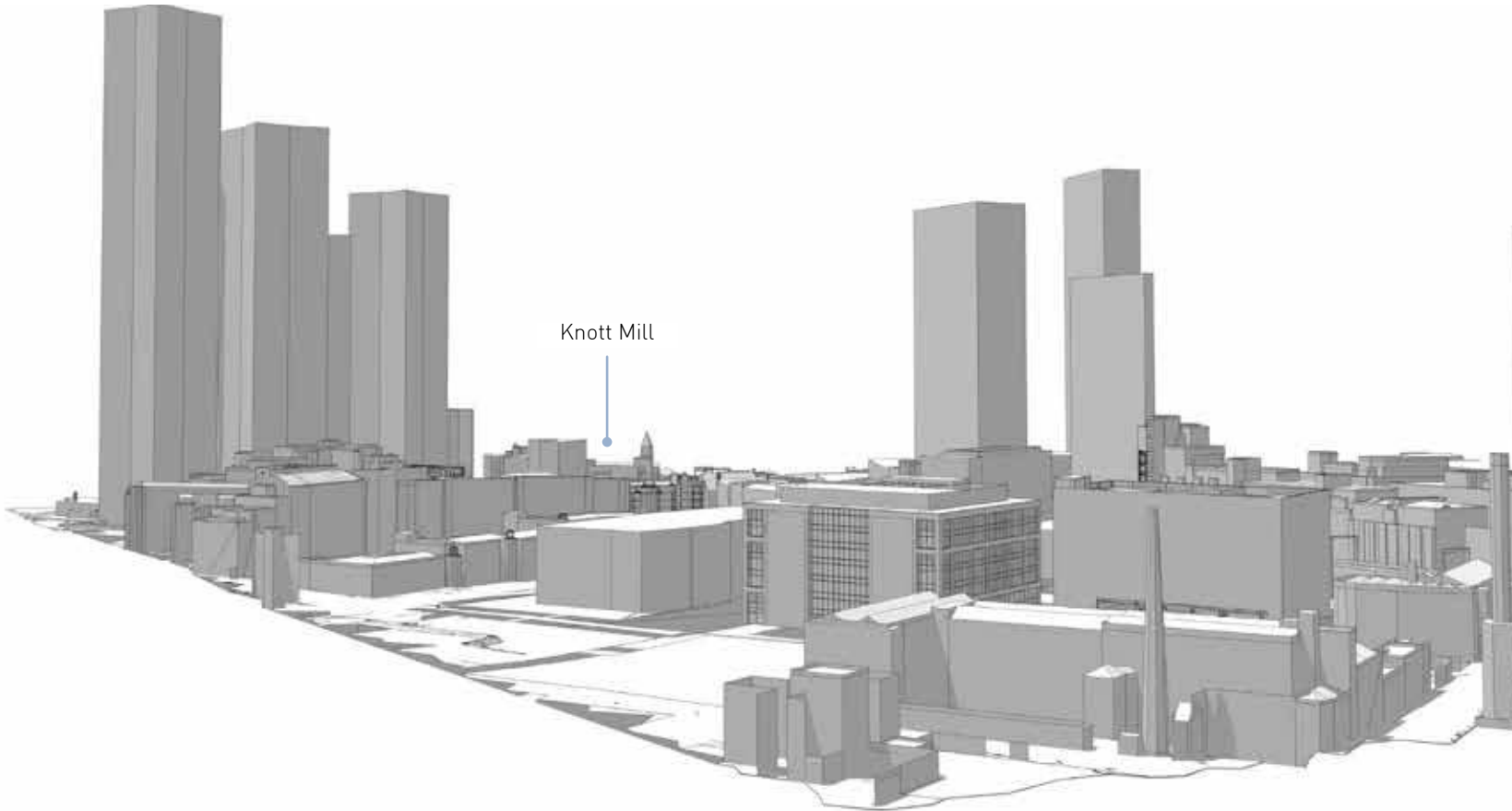
The Study Area is somewhat of an isolated neighbourhood when compared to the majority of its immediate neighbours where significant regeneration activity is taking place. The Study Area can be generally characterised as being of mixed character given the remaining industrial buildings and piecemeal recent development. Historically, the narrow street pattern resulted in a relatively dense grain. Presently, underdeveloped and vacant sites diminish the quality of the streetscape.



Street network



Figure/ground



Existing buildings and developments under construction at southern edge of city centre

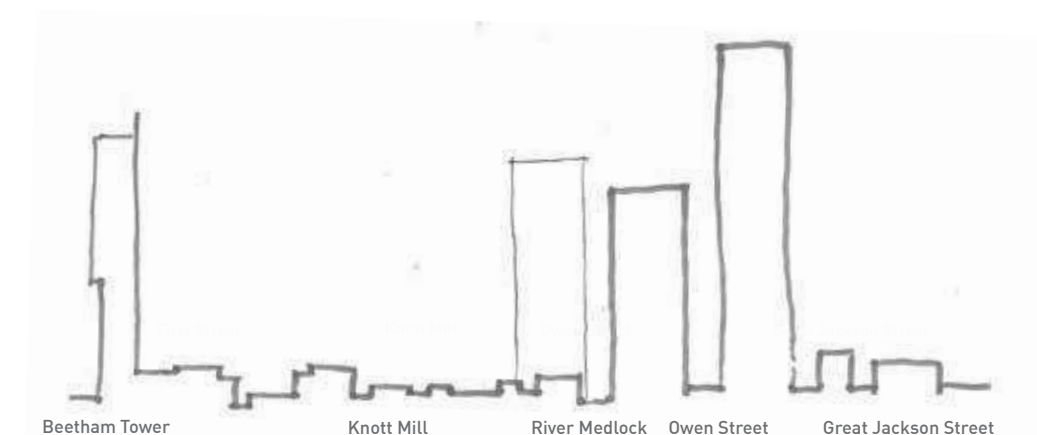
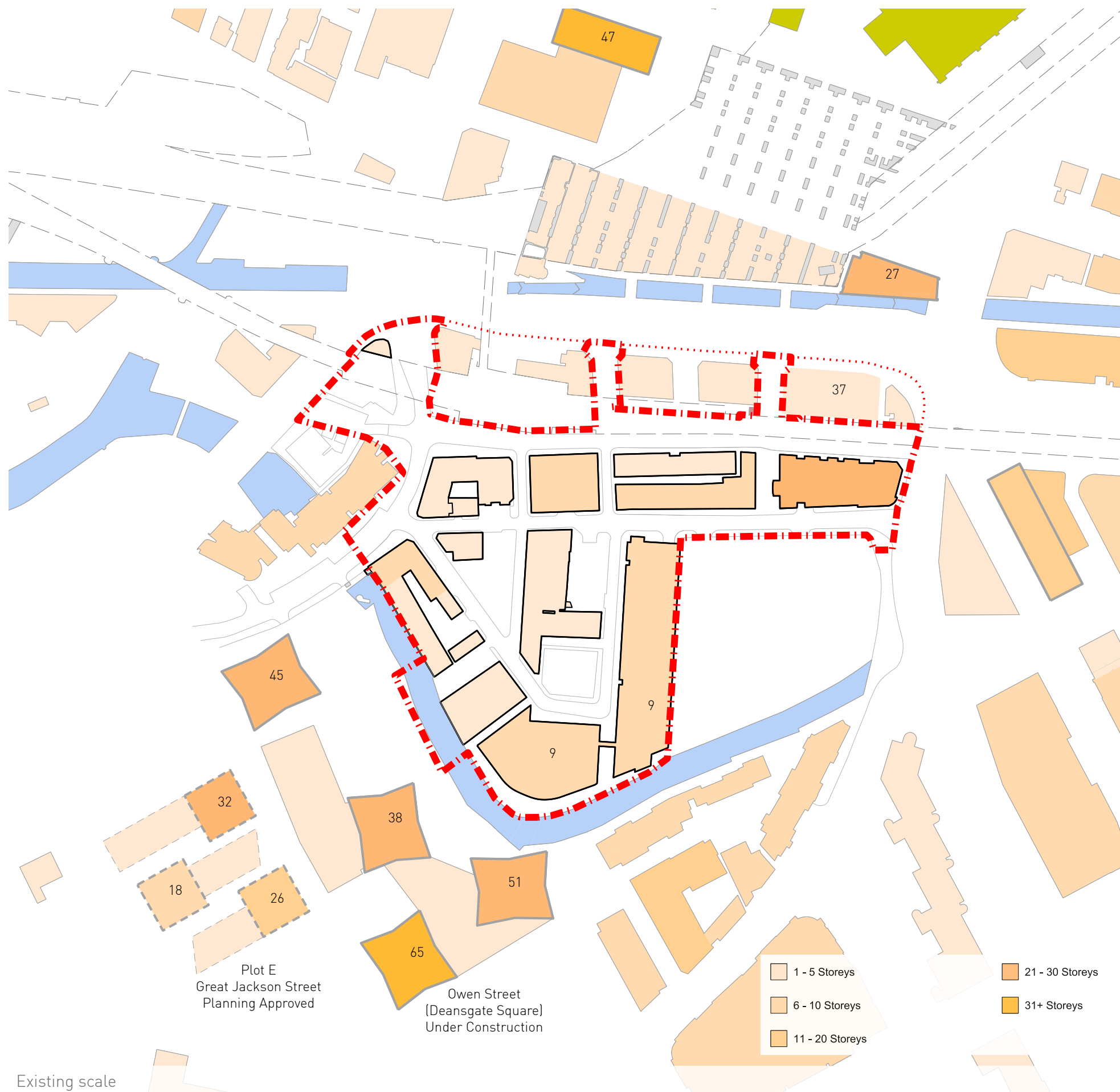


## 4.12 Height and Density

### 4.12.1 OVERVIEW

At present, a lower-rise core of predominantly former industrial buildings exists at the heart of Knott Mill with the larger and more dense developments of Hill Quays and Ropeworks located to the east of the Study Area.

Adjacent City Council Strategic Frameworks, particularly the Great Jackson Street and Whitworth Street West Frameworks, indicate that the southern edge of the city centre will change significantly in the very near future with several higher density and significantly increased-scale developments currently under construction.



Cross section through south of city centre



4.0 STUDY AREA APPRAISAL

4.13 Gateways, Landmarks and Nodes

4.13.1 OVERVIEW

The nature of the Study Area’s boundary conditions mean Knott Mill is largely inward looking. Gateways into Knott Mill are at the southern extent of (old) Deansgate, Hewitt Street and Little Peter Street and from the parallel north-south streets off Whitworth Street West that run under the railway arches: Bugle Street, Rowendale Street, and Cameron Street. None of these gateways can be considered to be significant. Deansgate Station and Atlas Bar have some presence at the Study Area’s north-western boundary and have the potential to frame the principal gateway to Knott Mill and Great Jackson Street.

4.14 Public Spaces

4.12.1 OVERVIEW

Whilst active public amenity space within the Study Area is very limited, there are a wealth of public open spaces within easy access.

Principally, there are two existing areas of public open space close by at First Street and the Castlefield Basin. The proposals for Deansgate Square’s public plaza will deliver another significant public space in close proximity.

The Great Jackson Street Framework aims to make the Deansgate Square public space one of a series of linked public spaces which ease the connection to Hulme Park, bringing this important amenity closer, in terms of perception to the city centre.

4.15 Significant Views and Vistas

4.15.1 OVERVIEW

There are limited significant views and vistas into and towards the Study Area as a result of the nature of Knott Mill’s boundary conditions which comprise largely of viaducts and recent impermeable developments. However, the view from Deansgate beneath the railway arches towards Knott Mill is the most significant and should be considered carefully.



First Street



Castlefield Basin



Castlefield Bowl



Roman Gardens



St John's Gardens



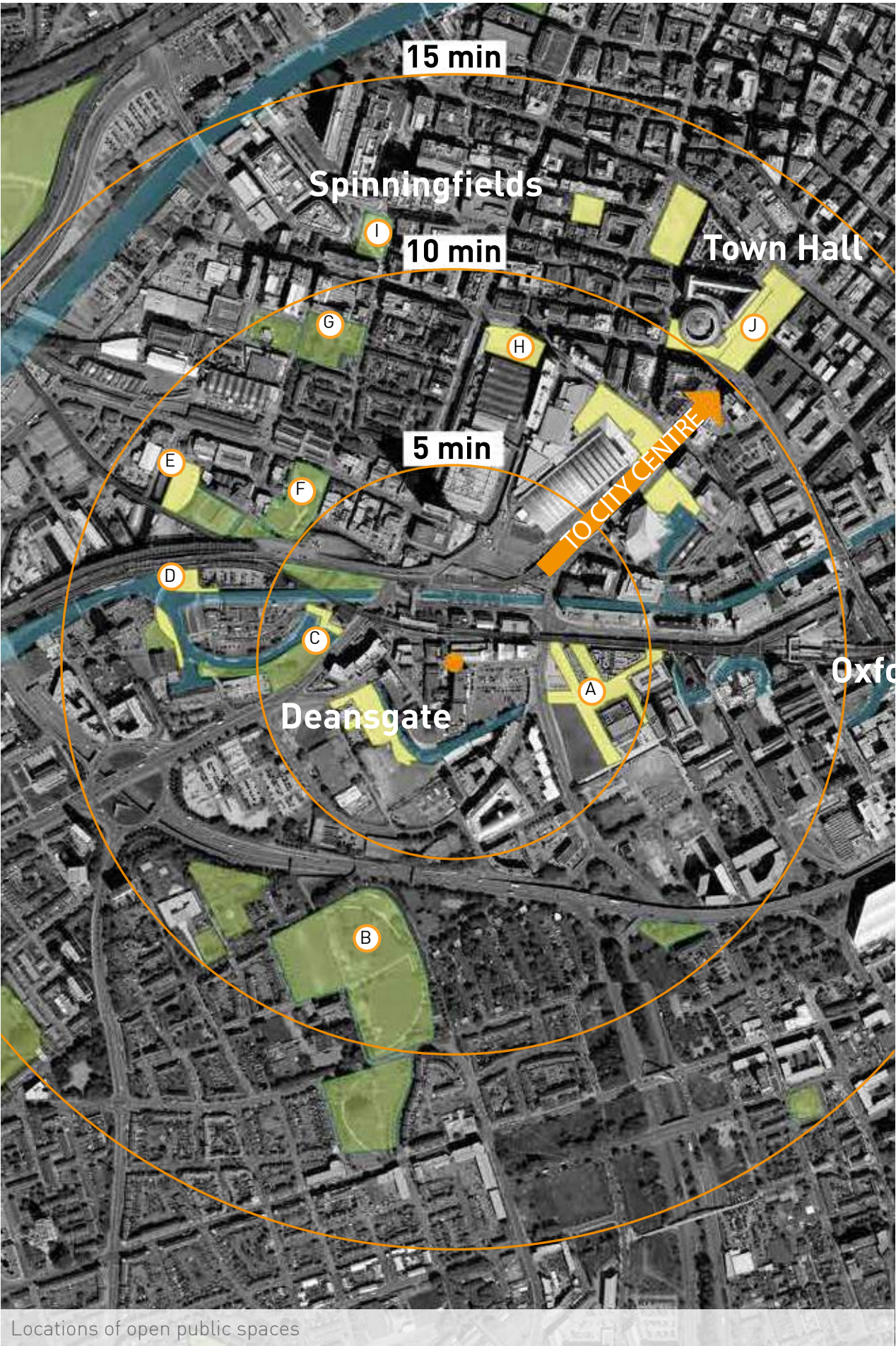
Great Northern



Spinningfields



St Peter's Square



Locations of open public spaces



## 4.16 Active Frontages

### 4.16.1 OVERVIEW

Active frontages are relatively mixed within the Study Area and directly impact on the townscape of the area. There are some primary active frontages along Jordan Street and Commercial Street. These take the form of contemporary floor-to-ceiling glazed ground-floor office space with residential development above. There are also some relatively poor-quality frontages, particularly at the northern part of the Study Area along Hewitt Street where there are several blank elevations which feature very few openings to the pavement from the activity behind.

Whilst this positive aspect of urban design is lacking in some areas, recent developments both within the Study Area and surrounds demonstrate that active frontages should be encouraged as they support services and facilities for the local community in the area. Active frontages can be achieved in the Study Area given that most of the units front the pavement line.

## 4.17 Topography

### 4.17.1 OVERVIEW

Topographically, the Manchester Conurbation as a region is within an undulating lowland basin. The Study Area lies directly north of the River Medlock and generally slopes gently from the river up to Whitworth Street West.

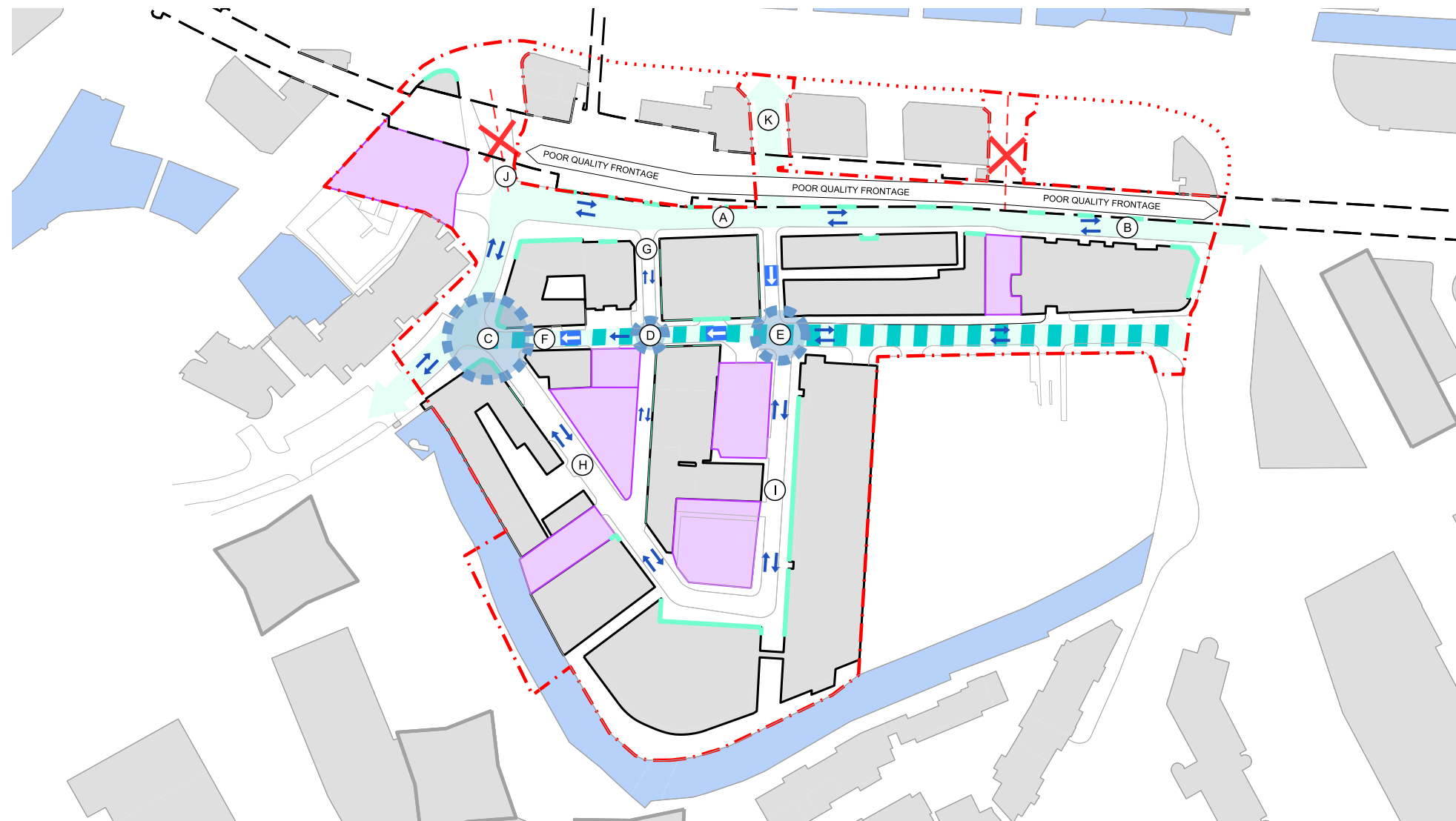
## 4.18 Summary of Constraints

### 4.18.1 OVERVIEW

The Constraints diagram provides an overview of the key constraints identified through the existing site and townscape analysis, which the Framework Development and Urban Design Principles look to address in establishing a framework for the future regeneration of the Study Area.

The constraints include:

- Poor quality frontages to Hewitt Street;
- A poor-quality junction at Little Peter Street off Deansgate;
- Standing traffic along Little Peter Street;
- Narrow lanes leading to confusion; and
- A number of areas that cannot be readily accessed by a vehicle.



(A) (B) Poor quality frontage to Hewitt Street	(G) Narrow lanes lead to confusion	(K) Through routes to Whitworth Street West	Private Land	Traffic Flow	Arterial Route
(C) (D) (E) Poor quality junctions at Little Peter Street	(H) (I) Loop route from Commercial Street to Jordan Street		Primary Active Frontages	One way traffic	Standing Traffic
(F) Standing traffic along Little Peter Street	(J) Dead end condition to Deansgate		Secondary Active Frontages	No through road	Poor Quality Junctions (proximity too close)













# 5.0 FRAMEWORK DEVELOPMENT AND URBAN DESIGN PRINCIPLES

## 5.1 Introduction

### 5.1.1 OVERVIEW

The purpose of this section of the document is not to prescribe definitive forms of development but rather to guide future development and provide a series of strategies to inform the future development of Knott Mill.

The masterplan strategies will be used as a material consideration in the consideration of future planning applications by the local planning authority for all sites within the study area to ensure proposals are developed in a co-ordinated and integrated manner.

In response to increasing pressure upon the study area imparted by adjacent development areas, an illustrative masterplan has been developed which proposes a vision for the future of Knott Mill informed by a detailed appraisal of the Study Area.

## 5.2 Approach to Study Area

### 5.2.1 OVERVIEW

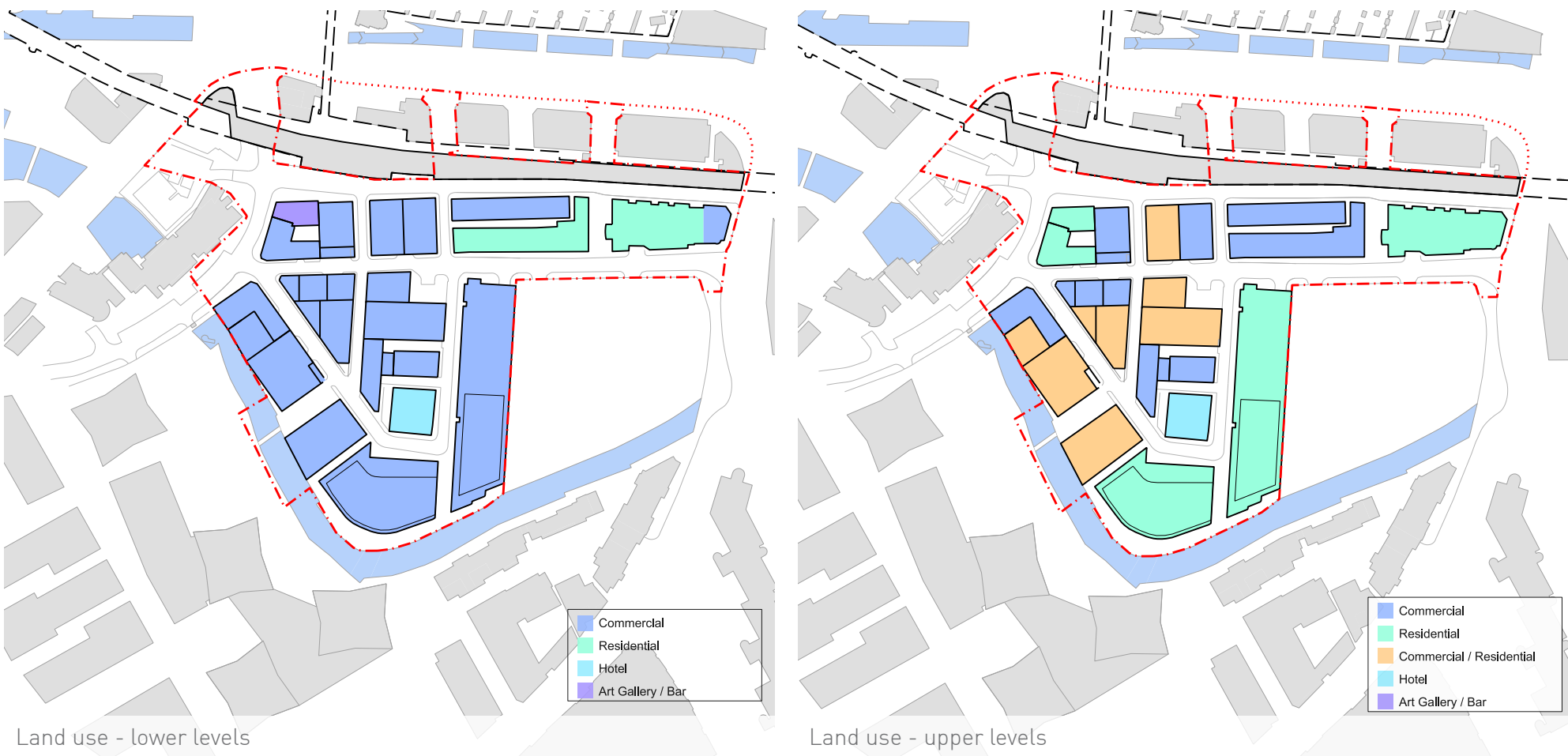
As a result of current and emerging development in the vicinity of Knott Mill, the notional boundary of the city centre is gradually extending south causing increasing pressure on the study area as a potential location for new development. In conjunction with this pressure, the following factors have emphasised the need for this illustrative masterplan;

- Under-utilised and vacant sites.
- Fragmented land ownerships.
- Limited connections to established and emerging adjacent development areas.
- Scale of emerging surrounding developments and overall relationship to southern edge of city core.

Future developments within the study area should continue to accommodate a mix of commercial and residential uses.

As a city centre neighbourhood, typologies should respond sensibly to the available development plots, the existing street pattern and the character and density of the surrounding development areas.

The illustrative masterplan establishes a set of strategic development principles with a focus on improving the legibility of the study area within its surroundings and on the role Knott Mill can play in enhancing the connectivity between the city centre and new communities to the south and east thereby becoming an integrating link between those sites and the city centre.





### 5.3 Core Development Principles

#### 5.3.1 LAND USES

The study area should incorporate a mix of scales of workspace together with a mix of types of high quality residential accommodation. This mix of uses will combine to create a distinctive sense of place and neighbourhood contributing further to the life and vitality on weekdays and evenings.

Economic analysis suggests that future housing need is fuelled by the 20-39 year old age group which will result in developments which are predominantly apartment led. Alongside this, there is a need to ensure that living space is created which can cater for a range of domestic situations, from flatshares to family living, which is increasingly prevalent in the city centre.

Ground level workspace will provide an alternative way of providing active frontages to the street where residential, retail or food and beverage uses might not be appropriate or where there are concerns about viability.

On the upper floors, workspaces should be designed to provide a range of scales of space, with the aim not only of attracting new businesses into the area but also of providing space into which the existing business community can grow.

Knott Mill is one of the city centre's relatively quieter residential neighbourhoods and any new uses need to respect this. Any night time uses such as bars could be accommodated only in appropriate locations.

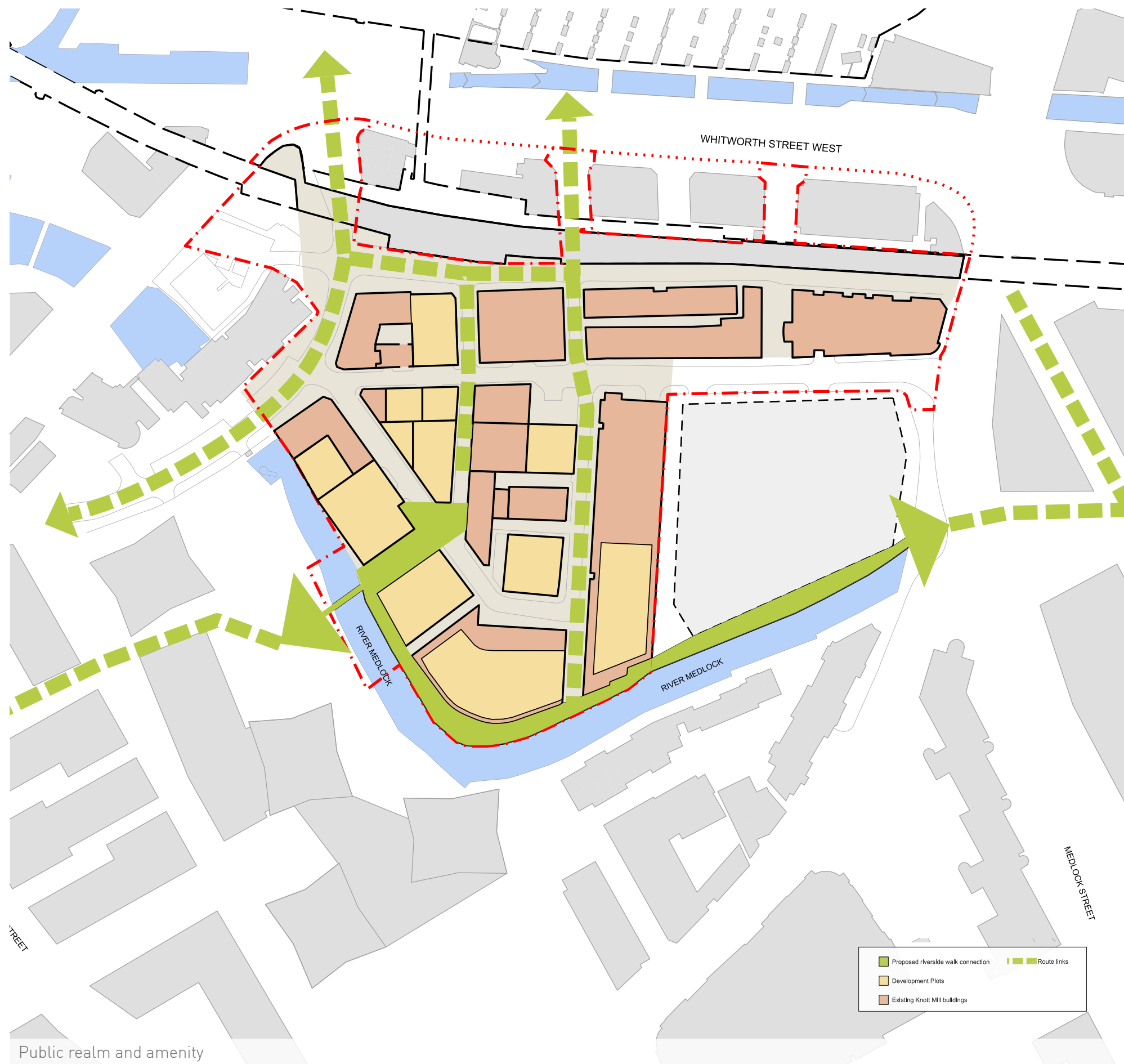
#### 5.3.2 CREATING A SENSE OF PLACE

Commercial workspace uses at ground level with residential or commercial use on the levels above will be particularly encouraged amongst the larger land plots along the southern edge of Commercial Street.

Flexible leasing strategies should be used where appropriate to encourage a mix of uses involving independent operators and start-up businesses building upon Knott Mill's reputation as a hub for creative industries.

Despite the prevalence of surface car parks and under utilised sites which provide no positive contribution towards the local townscape, there are a number of key character features within the study area;

- Buildings which add character to the area including; The former St Peters School (Boardwalk), 8 Hewitt Street, 10a Little Peter Street, 5 Constance Street and 4 Jordan Street.
- The River Medlock and the listed flood gate structures
- The road network within the study area is made up of regular setts which serve to unify the appearance of the streetscape. Where these setts need to be temporarily removed for maintenance/construction they should be stored safely and re-installed as soon as practically possible.





# 5.0 FRAMEWORK DEVELOPMENT AND URBAN DESIGN PRINCIPLES

## 5.3.3 PUBLIC REALM AND AMENITY

The Study Area is well located in terms of nearby amenity resources including; the Castlefield Basin, Hulme Park, First Street, Deansgate Square’s public plaza, St John’s Gardens as well as other further afield accessed via the nearby metrolink network.

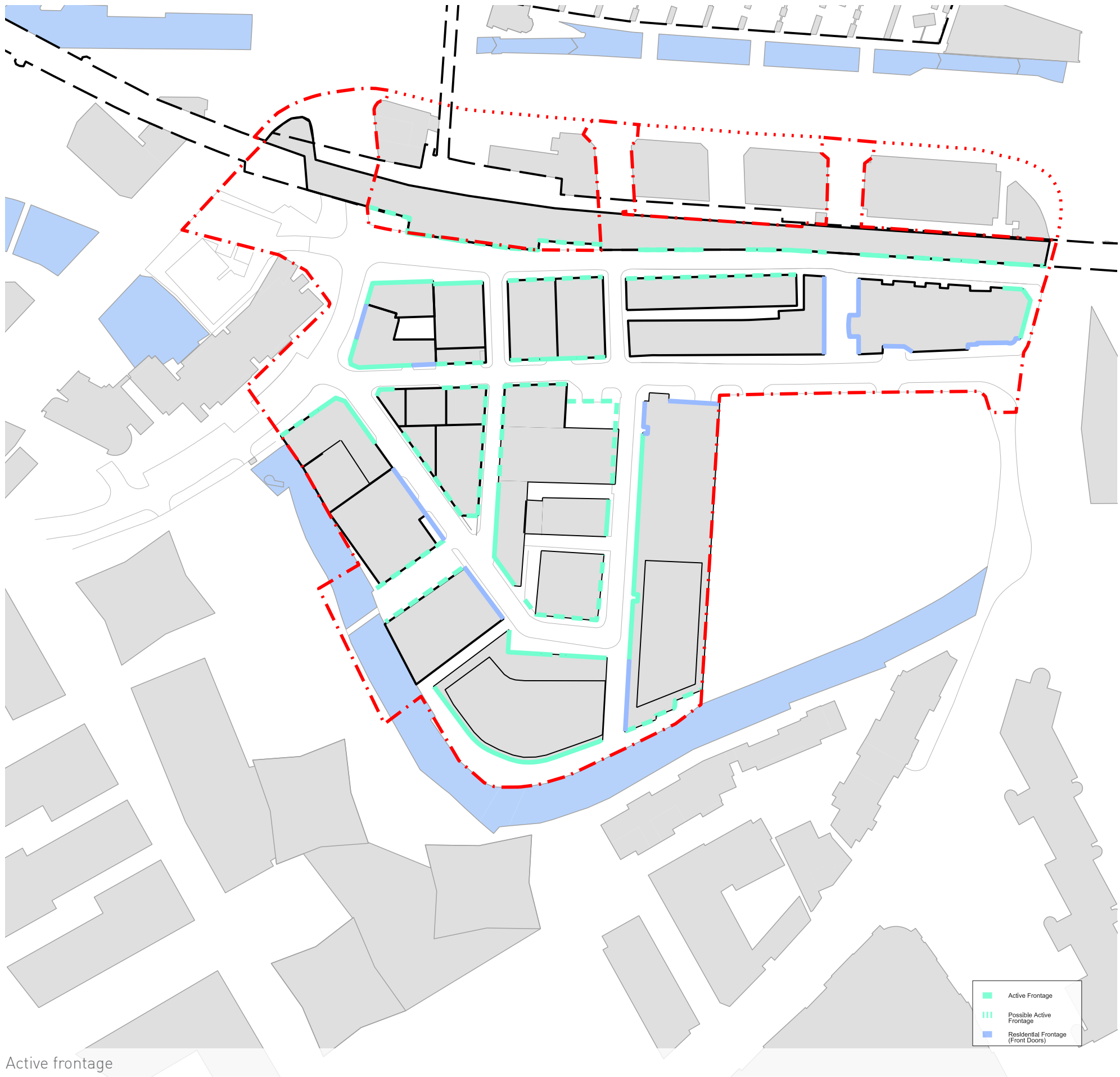
The Study Area itself provides limited existing public amenity and the quality of the urban realm is low. The area would benefit from a public space at its heart, however the combination of small land parcels and mixed ownerships mean that there are limited opportunities to create significant new open public space in Knott Mill.

The City Council will encourage developers to explore opportunities to maximise green infrastructure across the masterplan area, through the provision of planting and green or brown roofs.

The large open space alongside Deansgate Railway Station beneath the railway bridge serves as a gateway to Knott Mill, the Great Jackson Street development area and the city centre, and has the potential to become a welcoming, landscaped, vehicle-free space which would benefit from de-cluttering, installation of new street lighting alongside restoration and repair works to the railway bridge.

Enhanced and extended connections along and across the River Medlock would enable visitors and residents of Knott Mill to more easily enjoy large scale public amenity spaces at First Street and Deansgate Square, and the illustrative masterplan proposes a small urban space be created at the junction of Constance Street and Commercial Street at the confluence of a number of these routes.

The routes surrounding the study area benefit from some existing cycle paths although there are limited cycle parking facilities available within the existing Study Area. New residential and office development should incorporate cycle parking facilities and public cycle parking should also be provided within new public realm.



### 5.3.4 CONNECTIVITY AND PEDESTRIAN ENVIRONMENT

The potential provision of a pedestrian/cycle bridge across the River Medlock would connect Deansgate Square’s public plaza, the Great Jackson Street developments, First Street and the city centre.

Continuation of the existing, but limited, river walk provided as part of the Hill Quays development west as far as the new bridge and east across the Little Peter St car park site to City Rd would open up the undervalued asset of the river.

At present, the Deansgate-Oxford Road railway viaduct acts as a physical and psychological barrier to pedestrian movement between the rest of the city centre and Knott Mill. The quality of the routes through the viaduct from Hewitt Street in terms of pedestrian experience is poor and pedestrian movement and orientation is not a welcoming or positive experience. The pedestrian environment along Hewitt Street, Bugle Street, Rowendale Street and Cameron Street should be improved.

As the gateway between Knott Mill and the city centre, the large open space alongside Deansgate Railway Station beneath the railway bridge should be enhanced and a more attractive and easily navigable pedestrian environment should be created through the removal of vehicular access which currently interrupts free pedestrian movement along this key axis.

### 5.3.5 CAR PARKING

The study area accommodates several surface car parks which are generally in private ownership together with a number of on-street pay and display public carpark spaces as well as the adjacent large public surface carpark on Little Peter Street. As the area is developed further, the Core Strategy requires all development to provide appropriate levels of parking with each development proposal to be considered on a case-by-case basis.

New development proposals should therefore be accompanied by a car parking strategy which considers the promotion of alternative, more sustainable forms of transport and integrates any on site car parking into proposals in a manner so as not to detract from the character or animation of the street.

### 5.3.6 SUSTAINABLE AND SAFE DEVELOPMENT

The core principles set out above are key to ensuring that Knott Mill continues to develop as a community that is able to thrive in Manchester’s low carbon future.

As a first principle, the masterplan seeks to retain and reuse the existing buildings which are key to the area’s distinctive character.

All new development in Knott Mill should be carefully considered in order to help to facilitate the changes in energy efficiency, energy generation and energy usage needed to meet the target set in the draft Manchester Zero Carbon Framework, for the city to be zero carbon by 2038.

In particular, development proposals should:

**Promote sustainable forms of travel** through the enhancement of connections and the design of streets and public spaces which assist in making walking and cycling the natural options for those making short journeys around the city centre. Knott Mill’s city centre location and extremely close proximity to key public transport connections will assist in limiting car journeys and the new connections proposed in this document should be designed to make walking and cycling the preferred way of travelling into the city centre or to adjacent neighbourhoods or community facilities. The infrastructure should be provided to promote low and zero carbon forms of transport. New development should promote a reduction of reliance on car usage, eg through the provision of car clubs. Charging points should be provided within new developments and, ultimately, within the public realm. Cycle storage should be provided within new developments and cycle parking locations should be designed into any new public realm.

**Adopt high standards of building design** to minimise requirements for heating and cooling. Building orientation should be carefully considered in relation to solar gain and heat loss and buildings should be designed, built and insulated to the highest standards.

**Include heating and cooling systems which have been designed with low/zero carbon in mind.** Designers should review opportunities for the generation of renewable low or zero carbon energy. Sources could include photovoltaics, solar or heat pumps. Through collaboration between landowners it may be possible to develop district heating networks serving the larger plots. These should be adaptable over their lifetimes to accommodate alternative power sources as the availability of zero carbon energy sources increases.

**Sustainable Drainage Opportunities.** Development proposing new public realm should allocate provision for the management of surface water through the use of sustainable drainage systems with multi-functional benefits as part of a high quality green and blue water environment. Development with such features should consider the topography of the site to understand any naturally occurring flow paths and any low lying areas within the proposal where water will naturally accumulate. The discharge of surface water via the River Medlock should be explored as a minimum.

New development will be expected to incorporate exemplary Sustainable Drainage methods and follow the surface water hierarchy. Applicants will have to submit clear evidence of why each option within the hierarchy has

been discounted. The expectation will be for only foul flows to communicate with the public sewer.

Approved drainage schemes will be expected to be supplemented by appropriate maintenance and management regimes for the lifetime of any surface water drainage schemes.

**Water Efficiency Measures.** The design of new development should consider the inclusion of water efficiency measures in the development of new buildings. New development can become more resilient to climate change by encouraging water efficiency measures including water saving and recycling measures to minimise water usage. Such a proactive approach is designed to mitigate and adapt to climate change, taking into account the long-term implications for water supply in the city.

**Secure by Design.** The principles of ‘Secured by Design’ should be followed to ensure developments contribute to the creation of a safe and secure environment accessible for all.

### 5.3.7 IMPROVING KEY INTERFACES.

**Old Deansgate** sits half in the Knott Mill area and half in the Great Jackson Street Strategic Regeneration Framework Area and is a key connection between Great Jackson Street, Knott Mill and Deansgate and improvements to the public realm would be welcomed.

The **Deansgate Quay** residential development has a frontage along the length of Old Deansgate facing into Knott Mill. Proposed developments within Knott Mill should ensure Deansgate Quay and its associated public realm is fully integrated.

Although outside of the masterplan area, the City Council and Knott Mill Association would support works to improve the appearance of the **River Medlock** (the Deansgate Square proposals as part of the Great Jackson Street SRF, include the provision of an access ramp to allow the Environment Agency to access and maintain the river bed). The **floodgate** adjacent to Old Deansgate, at the point at which the River Medlock enters the culvert which takes it below Castlefield, is a listed structure. The City Council and Knott Mill Association would support works to improve the visibility of the floodgate from Old Deansgate.

The **Hewitt Street** arches present an opportunity to provide active uses and these should be considered, provided that they do not result in the unnecessary displacement of existing businesses. Opportunities to redevelop these arches should be explored with Network Rail and the Arch Company.



## 5.0 FRAMEWORK DEVELOPMENT AND URBAN DESIGN PRINCIPLES

### 5.4 Urban Design Principles

#### 5.4.1 DEVELOPMENT TYPOLOGIES

New developments within the Study Area will respond appropriately to the form and size of development plots, the street pattern, the character, density and scale of surrounding developments and the target market for the study area. The typologies considered to be suitable for the Study Area are mixed use buildings containing residential apartments and duplex units together with commercial office/workspace with subdivisible, small or mid size floor plates.

#### 5.4.2 RESIDENTIAL AMENITY

Existing plot dimensions do not support the provision of courtyards or private outdoor amenity space so amenity should be provided in the form of balconies, winter gardens and accessible roof spaces where possible. New developments should create an environment where the amenity of residents and the environment within the site with regard to privacy, microclimate, noise, refuse management, safety and vehicular movement for example are maximised.

#### 5.4.3 ACTIVE FRONTAGES

Currently there are interrupted ribbons of active frontage. New development or refurbishment of existing buildings should be designed to support active frontages particularly along pedestrian desire lines to promote street life and to animate the street scene. Where the ground floor is intended to contain commercial or retail uses it should incorporate transparent glazing to promote street life and provide visual interest for pedestrians.

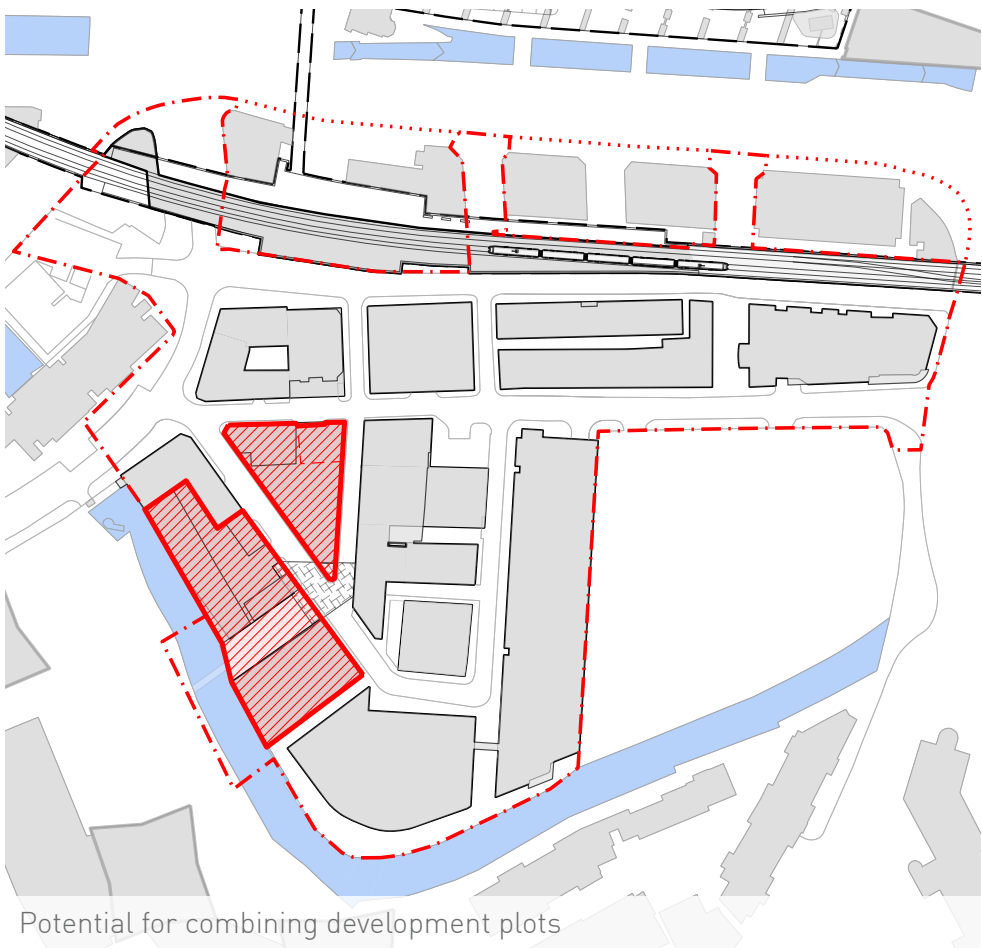
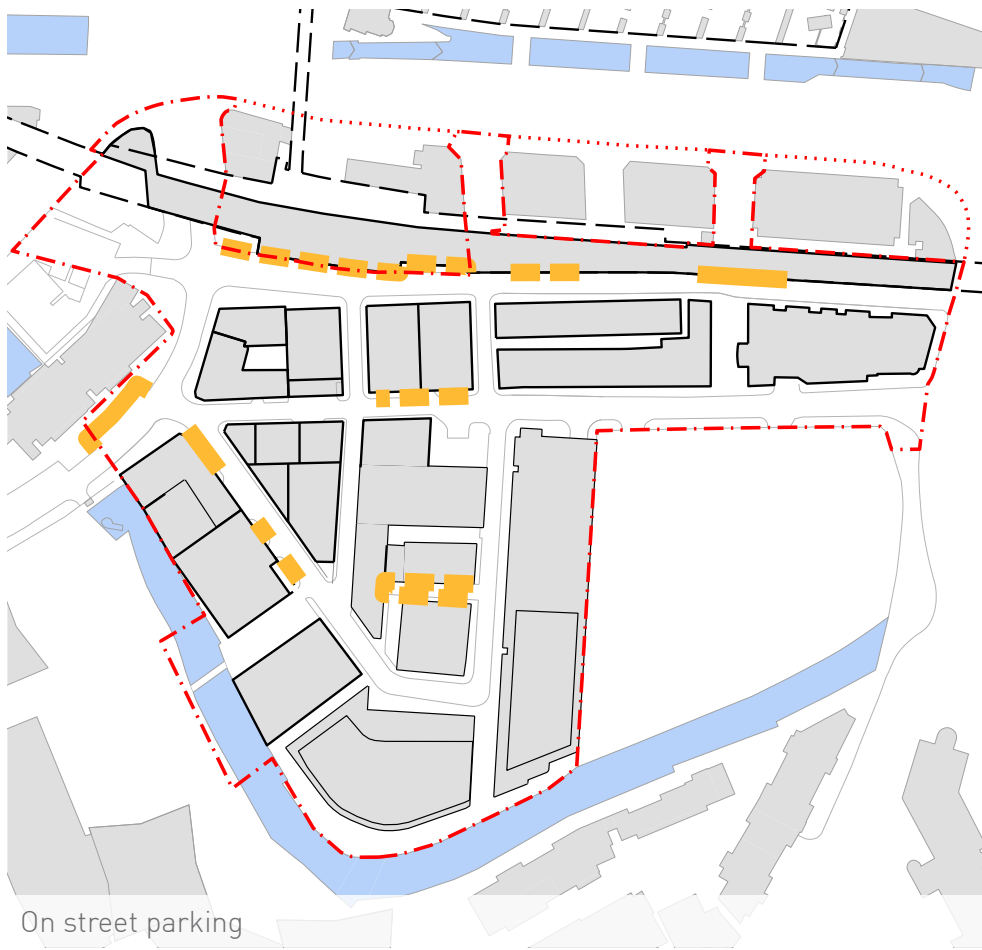
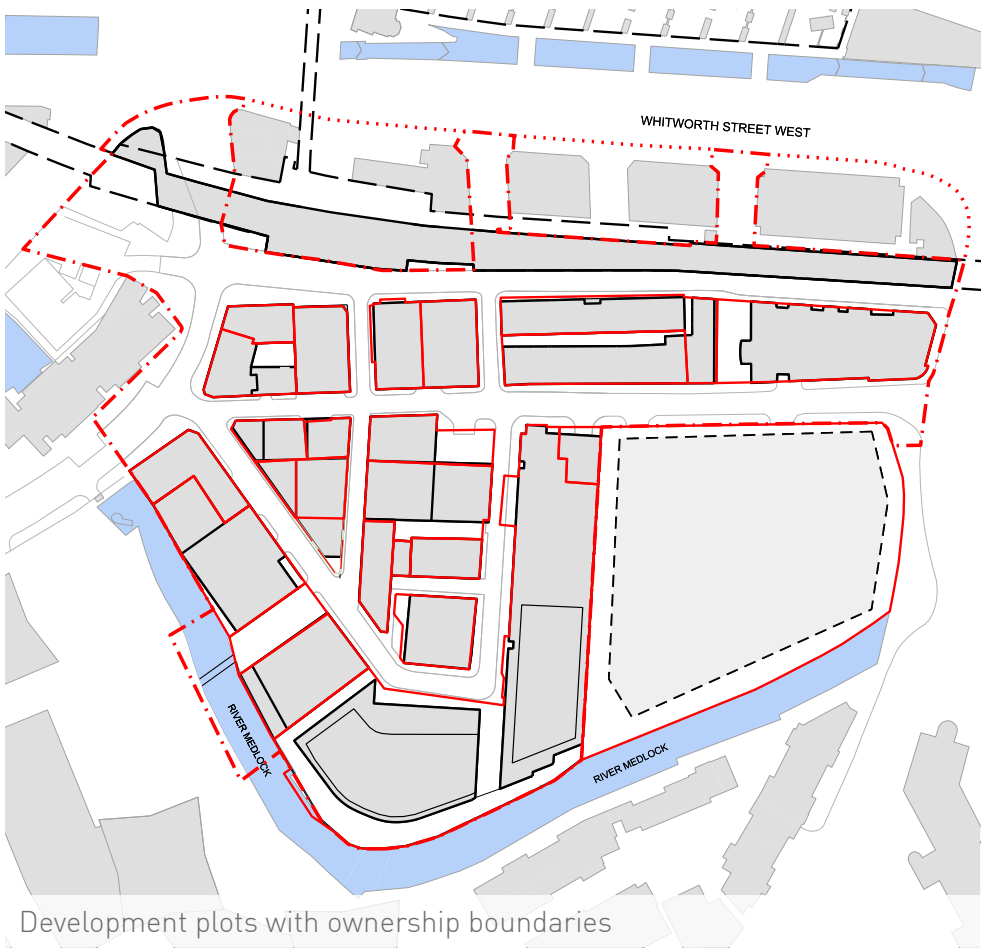
As mentioned above, the Hewitt Street arches present an opportunity to provide active uses and these should be considered, provided that they do not result in the unnecessary displacement of existing businesses.

Opportunities to introduce active frontages along the pedestrian walkway to the southern boundary of the masterplan area should also be explored.

#### 5.4.4 GREAT JACKSON STREET FRAMEWORK INTERFACE

The Study Area's principal connection into the surrounding street network is via (old) Deansgate, a route which will increase in prominence and see significantly increased pedestrian flows in the direction to-and-from the city centre via Deansgate. Public realm improvements including landscaping, and lighting particularly beneath the Deansgate Railway bridge will be required along this axis.

A significant linear, pedestrian route runs south-west to north-east through the Great Jackson Street Framework proposals. This route terminates at the large public plaza within the Deansgate Square development. This axial route has the potential to be extended via a bridge link into the Study Area at Commercial Street via the 4 Commercial Street and Commercial Wharf plots.



## 5.0 FRAMEWORK DEVELOPMENT AND URBAN DESIGN PRINCIPLES



Street hierarchy

### 5.4.5 FIRST STREET FRAMEWORK INTERFACE

Hewitt Street and Little Peter Street run perpendicular to the Albion Street interface with First Street at the eastern edge of the Study Area. A pedestrian crossing is in place at this location and this facilitates a pedestrian connection between Knott Mill and the Public Plaza at the heart of First Street.

The Little Peter Street carpark site falls outside the study area due to its inclusion in the First Street framework. This masterplan suggests that future development on this plot should accommodate an extension to the existing section of riverside walkway provided by the Hill Quays development as a means of creating an alternative new east-west connection across the study area.

### 5.4.6 STREET HIERARCHY

The predominant vehicular route through the study area takes traffic from Chester Road along Hewitt Street and onto Albion Road. This route is frequently used as a 'rat-run' at present and landscaping proposals which seek to discourage rat-running will be welcomed.

The 'core' of the study area, i.e. the Jordan Street/ Commercial Street/ Constance Street interfaces form a generally quieter, secondary vehicular route through Knott Mill and is generally used for access, servicing and drop-off only. The street hierarchy strategy seeks to maintain this more intimate core, free from the majority of through traffic but promoting the existing north-south routes which generally serve pedestrian movements. A public space at the junction of Constance Street and Commercial Street would assist in reinforcing this distinction between the quieter 'core' and the busy perimeter of Knott Mill.

### 5.4.7 PUBLIC REALM AND AMENITY

As part of the place-making strategy, enhanced public realm and amenity are to be provided. The illustrative masterplan indicates locations for delivery of these spaces and routes.

### 5.4.8 COMBINING DEVELOPMENT PLOTS AND CAR PARKING STRATEGY

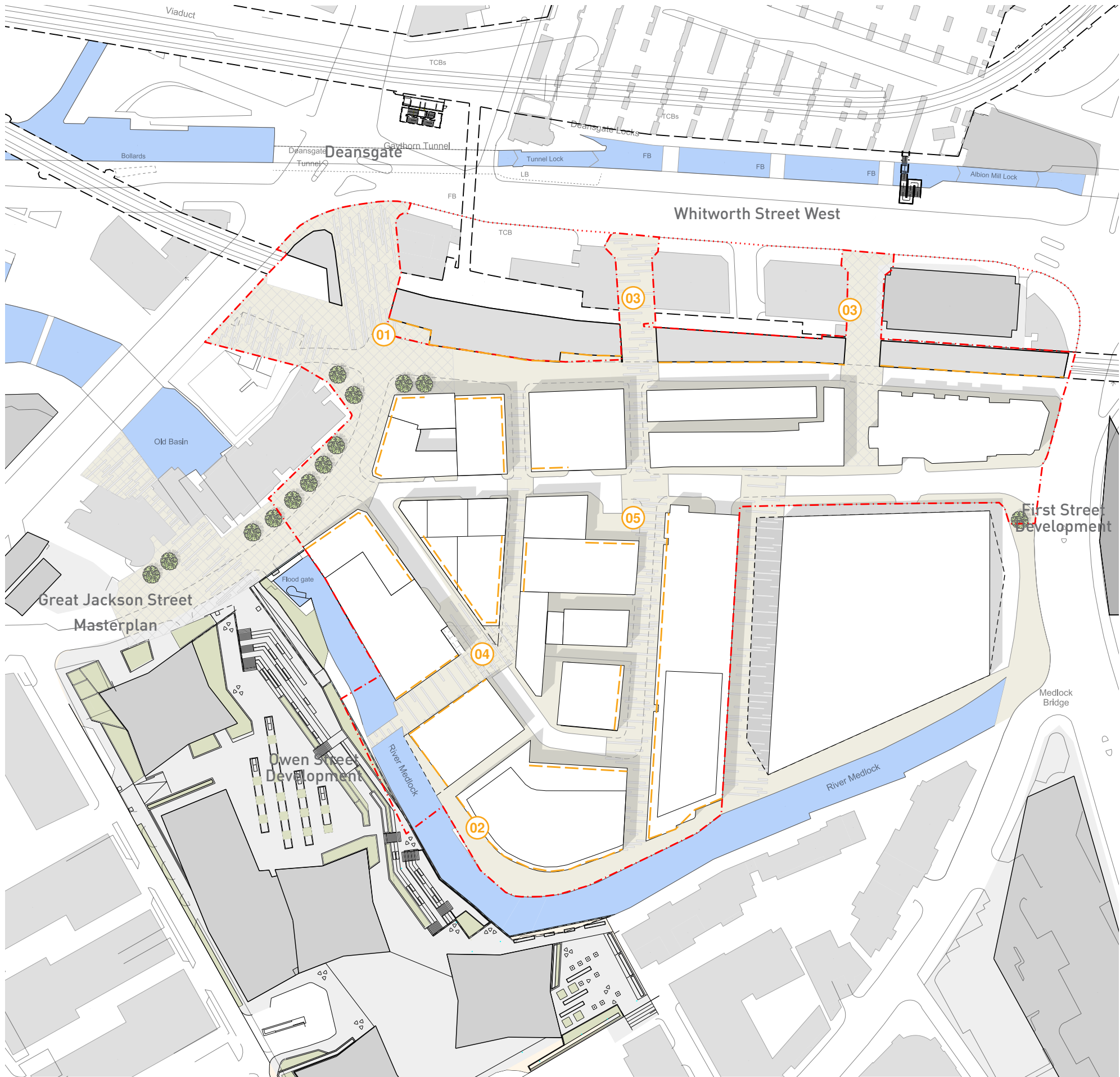
The fragmented nature and size of development plots could impede high quality, viable developments and limit the ability to deliver sensible on-site car parking arrangements. In addition, the flood risk may mean that the cost of delivering basement car parking would not result in delivery of affordable car parking spaces without prejudicing overall development viability. The larger plots along Commercial Street could provide sufficient capacity to incorporate car parking at ground level. Ground level parking would need to be wrapped in development to prevent dead frontage. Combining plots will be supported where it can deliver enhanced development outcomes and drive better public realm and more viable schemes.



5.4 Urban Design Principles

5.4.9 LANDSCAPE STRATEGY PLAN

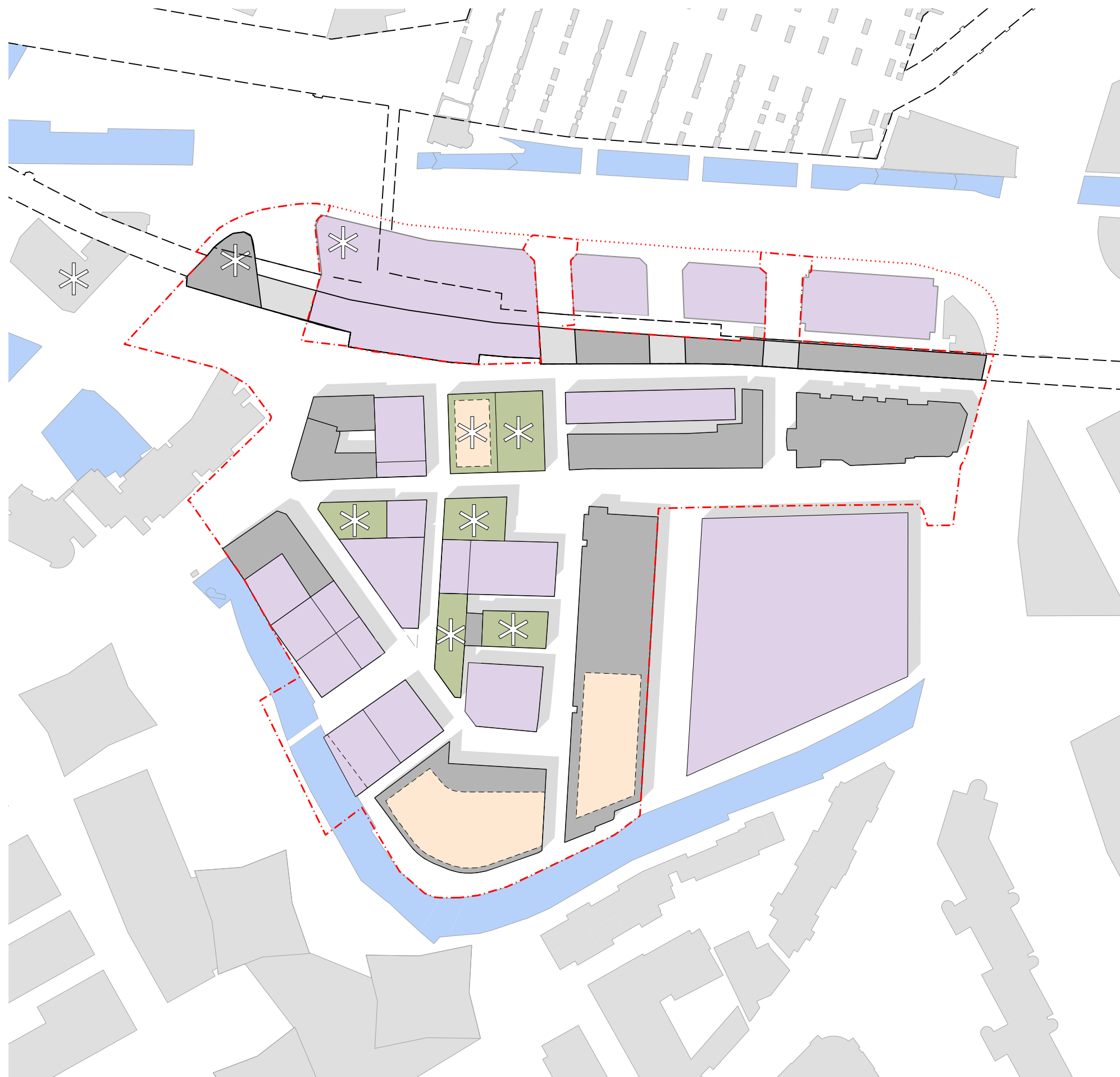
- 01 Deansgate / Gt Jackson St link - pedestrian priority
- 02 Owen St / First link - extended river walk and footbridge
- 03 Whitworth St West / Hewitt St link - shared surface / traffic calming
- 04 Commercial St / Constance St - new public space
- 05 Little Peter St/Jordan St - new public space



----- Active Frontages

## 5.4 Urban Design Principles

### 5.4.10 MASTERPLAN STRATEGY PLAN



- Future development opportunity (under utilised or vacant sites)
- Future rooftop extension opportunity
- Retained character asset
- Recently developed / unlikely to be redeveloped in near future
- \* Retained building of historic cultural value



5.4 Urban Design Principles

5.4.9 HEIGHT AND DENSITY OVERVIEW

Limitations on height and scale have been developed in response to detailed contextual appraisal of the area including those developments under construction and consented in the vicinity of the study area.

It is acknowledged that there is also potential for high density development on the sites within the zone of influence of the masterplan along the Whitworth Street West frontage, including the Deansgate station site.

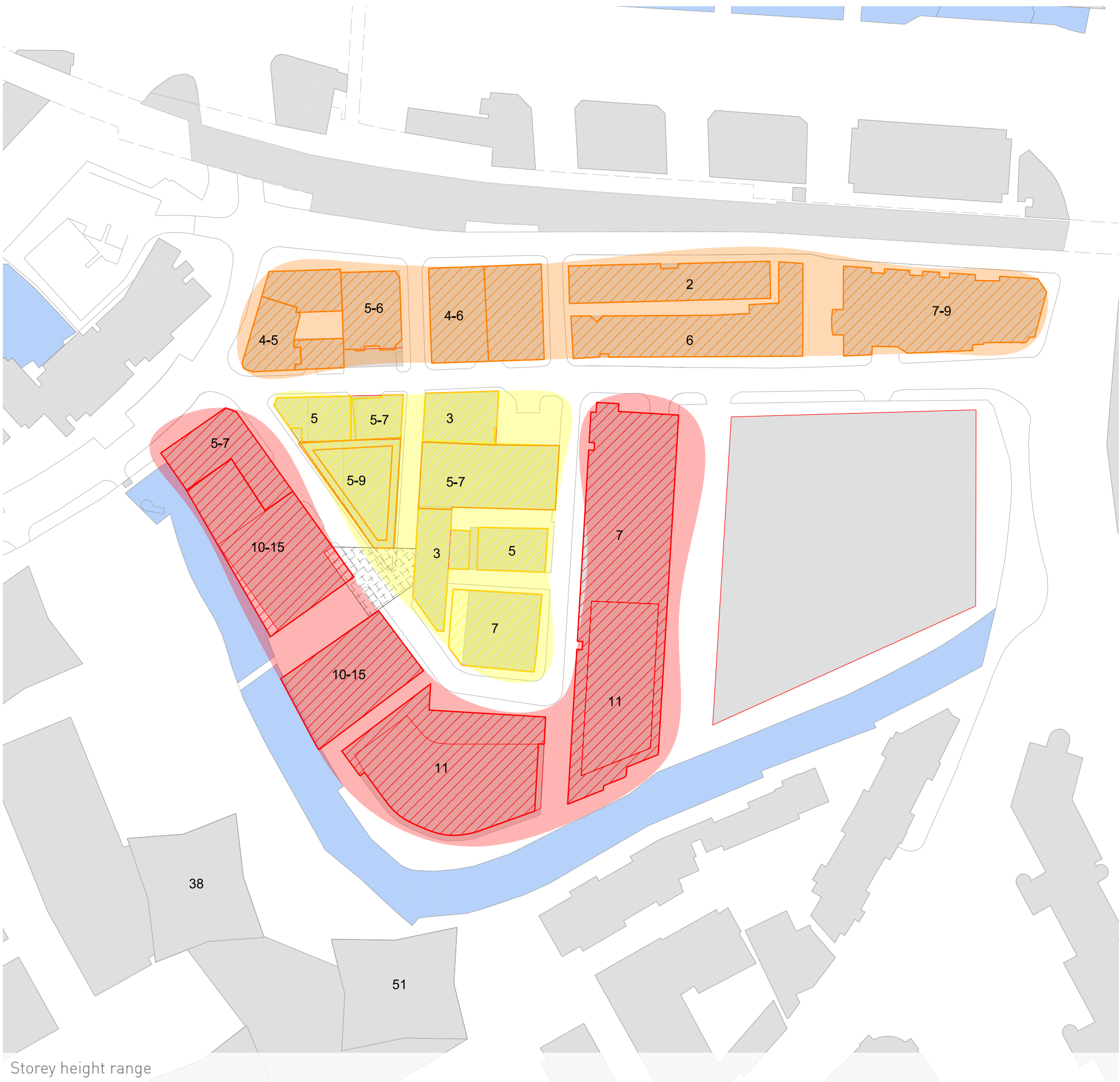
An economic and development appraisal alongside masterplanning analysis has provided a basis for ensuring the regeneration outcomes are deliverable and the strategies are focused on ensuring this strategic site is best utilised to create a neighbourhood of choice.

The starting point for the masterplan strategy is the retention and reinforcement of the historic street pattern alongside acknowledgement of buildings which contribute to the character of the area. By considering the future role Knott Mill will play in integrating wider developments into the city centre, there is an opportunity to create a permeable neighbourhood which builds upon on the existing character whilst enhancing the sense of place.

New development should draw upon and clearly reference the positive characteristics of adjoining neighbourhoods such as First Street, Great Jackson Street and the Castlefield Basin which have their own strong character and identity.

With regards to scale within the study area, three distinct zones have been identified;

- The Band (in orange) - The strip of midrise development along the northern edge of the study area. A mix of recently developed buildings either side of the character assets of 8 Hewitt Street and the former St Peter’s School.
- The Core (in yellow) - The central lower rise heart of Knott Mill which comprises the existing stock of historic buildings of most character value.
- The Fringe (in red) - The larger plots and large scale developments along the river edge capable of mediating in scale with the Owen Street and Great Jackson Street developments.





## 5.0 FRAMEWORK DEVELOPMENT AND URBAN DESIGN PRINCIPLES

### 5.5 Character and Materiality

#### 5.5.1 OVERVIEW

The elevational design of new developments within the Study Area needs to be carefully considered throughout the design development process to ensure the proposals are robust and deliverable. The material strategy should be developed to respond to the local industrial heritage, to reinforce the individual proposal's form and to ensure that the overall composition enhances the setting and character of Knott Mill.

Facades should be clad predominantly in brick or similar masonry products in order to provide a visual mass, anchoring the buildings into the industrial setting. Rather than replicating the dark red brick materiality of these existing buildings, alternative darker or lighter toned masonry products should be proposed to help establish a visual contrast with the existing buildings and to enhance the feeling of light and space within the tight-knit historic street pattern. Brick or other masonry finishes will give new buildings a monolithic hand-crafted quality.

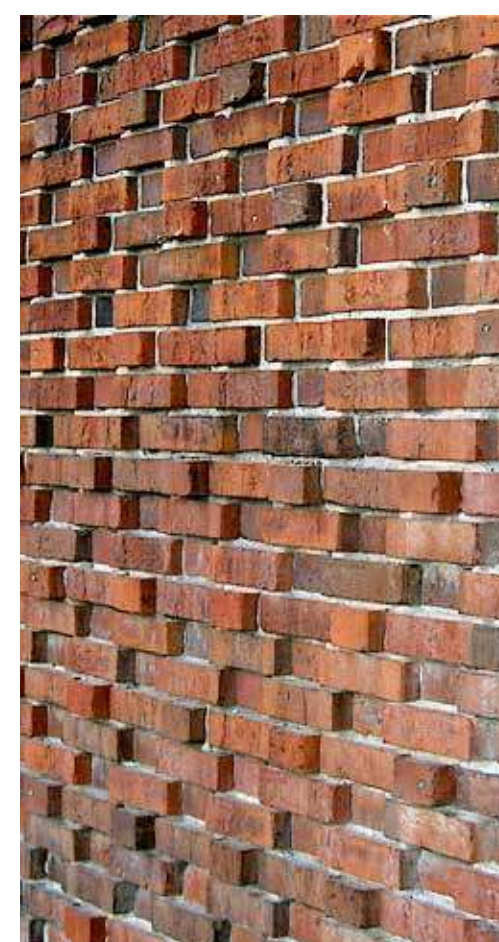
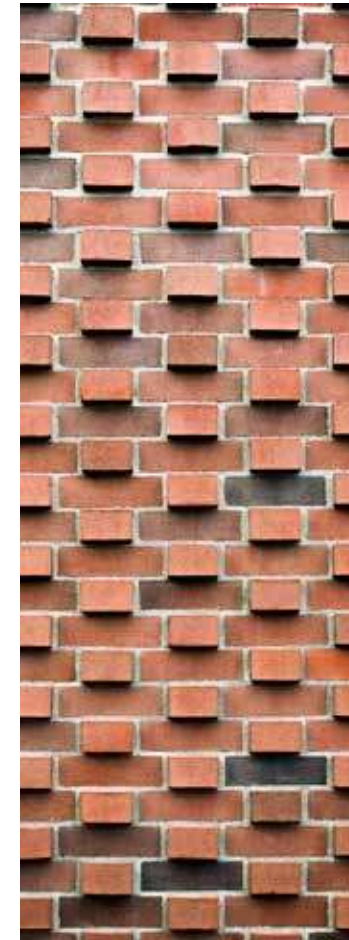
With regard to new taller buildings, darker masonry products should be used below a datum commensurate with the scale of neighbouring buildings with contrasting lighter materials used for the taller elements above.

Glazing should be accommodated within simple, minimal framing contained within the overall modular masonry frame and should promote natural day lighting and enhance views. The light and reflective properties of glass will provide a visual contrast with the heavyweight masonry frame. Increased use of glazing at ground levels will be particularly encouraged to enhance the sense of active use at street level.

Facades should consist of a series of equal brick piers or a module of recesses inset from the fenestration grid to create a sense of depth, increase actual and apparent mass and to set back the windows to increase privacy into bedrooms and living spaces when viewed obliquely given the tight knit nature of the existing streets.

A complementary palette of masonry based materials across new developments will ensure that Knott Mill retains a character which is distinctly different to that of adjacent large scale development areas - densely arranged built form commensurate with the monolithic nature of the remaining and previous historic industrial buildings. This will ensure that Knott Mill continues to be read as a distinct neighbourhood within Manchester City Centre.

The images of precedent on the following pages, in combination with the indicative sketches are intended to give an indication of the character of a development that could be created in Knott Mill, combining the existing buildings of character with complementary new developments.







Existing view - Little Peter Street/Commercial Street



Indicative sketch view - Little Peter Street/Commercial Street





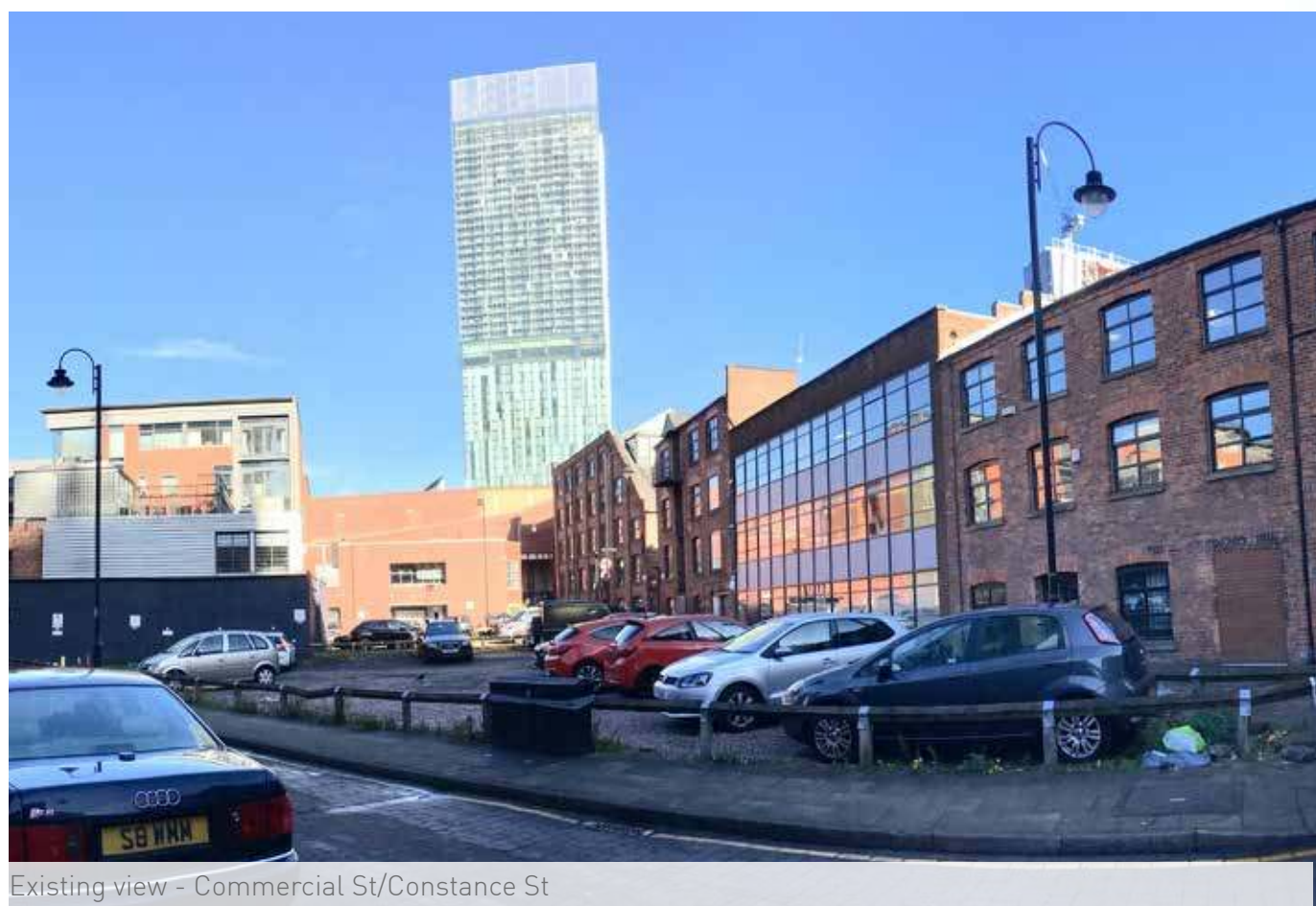
Indicative sketch view - Constance Street



Existing view - Constance St







Existing view - Commercial St/Constance St



Indicative sketch view - Commercial St/Constance St





Indicative sketch view - Little Peter Street

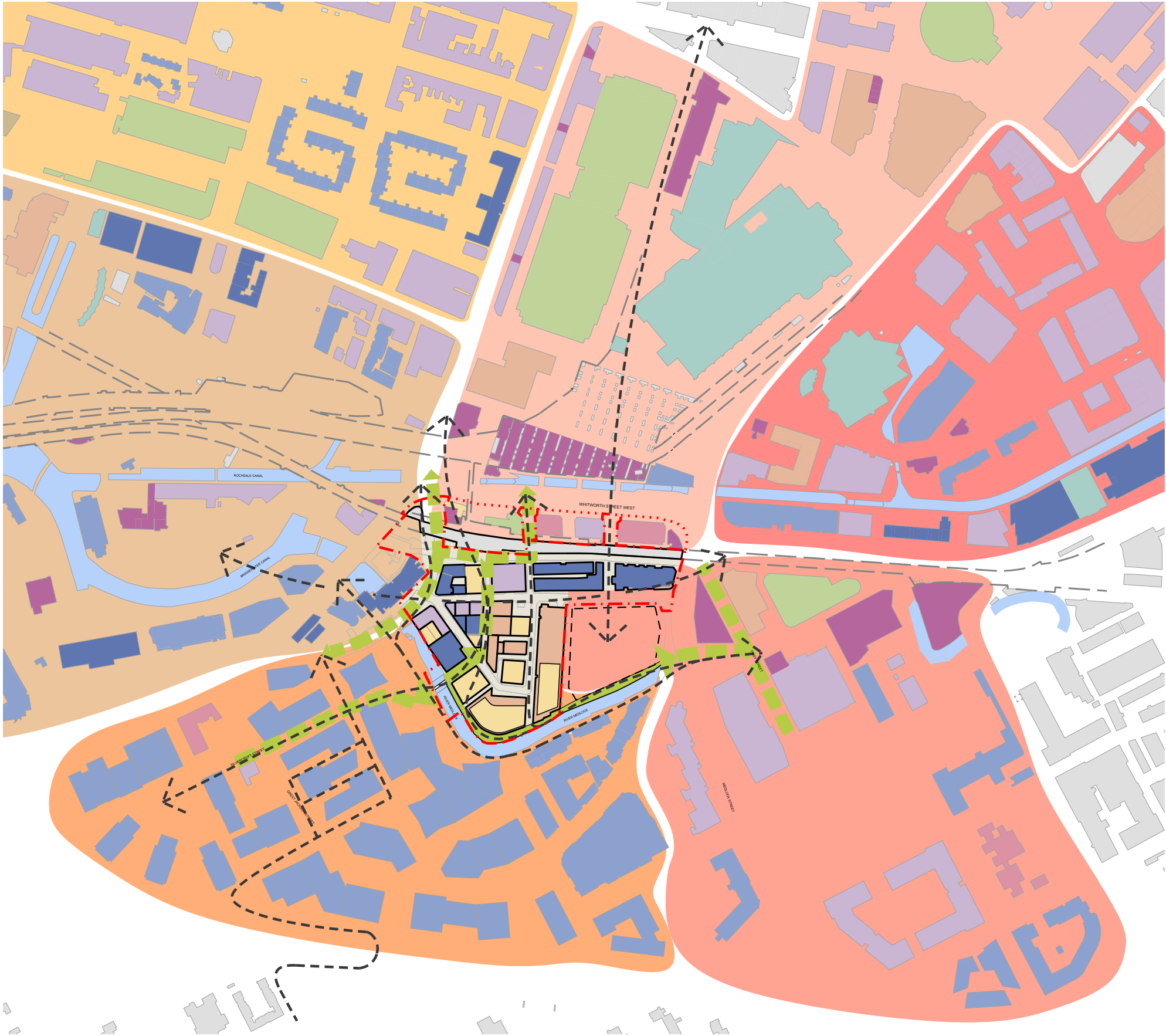


Existing view - Little Peter Street



5.6 Interconnections

**5.6.1** Plots within the Study Area should, where possible, make allowances in their arrangements to integrate with the surrounding neighbourhoods to create a series of interconnected urban spaces.



Interconnecting emerging neighbourhoods



## 5.7 Developer Contributions

**5.7.1** The south-western part of the city centre will play a significant part in satisfying identified current and future demand for new dwellings and commercial space in new neighbourhoods, which is required in order to support population and economic growth. The Knott Mill masterplan represents an opportunity to knit together a number of larger areas, such as Great Jackson Street, Whitworth Street West and First Street, and plan strategically for how the unique characteristics and circumstances of this particular area can contribute to the continued growth of this part of the city centre.

**5.7.2** The uses and architecture as well as the manner in which the more functional requirements (access, servicing, car parking etc.) are handled must therefore be of the highest quality. The contribution of new development, including extensions or alterations to existing buildings, to the quality of the environment in the area, as well as reinforcing pedestrian routes and desire lines from surrounding areas, will be of particular importance.

**5.7.3** Without an overall strategic approach to the area, and a drive for high quality design, the piecemeal redevelopment of the area would represent a significant missed opportunity in terms of reinforcing its existing characteristics and creating a more sustainable neighbourhood. A key purpose of this document will, therefore, be to provide a framework for the Local Planning Authority to ensure that any future development in the Knott Mill area is of the highest quality in terms of design and supporting public realm, highways and other community infrastructure in line with national and local planning policies and the Manchester Residential Quality Guidance.

**5.7.4** In accordance with adopted planning policies, including the NPPF and Policy PA1 of the Manchester Core Strategy, it therefore follows that in order to secure a sustainable future for the area and deliver a neighbourhood of choice, the Local Planning Authority will utilise all reasonable resources and mechanisms to secure appropriate contributions from new development on a site-by-site basis, either directly or in the form of financial contributions where appropriate, that will allow public realm and other community infrastructure to come forward in tandem with the delivery of development sites. This approach will help achieve quality outcomes for the neighbourhood and underpin the vitality and viability of the area.

**5.7.5** In order to maximise the sustainable, high quality regeneration outcomes set out in this document, and for the area to fulfil its potential, it is essential that a delivery strategy is developed which recognises the Study Area's strengths and key attributes and the significant opportunities.

**5.7.6** The purpose of the masterplan is to provide an overarching strategic approach to the regeneration of the Knott Mill area, and to set the parameters and intent for future development. Given the likely regeneration timescales, it is important to recognise that the design principles set out within this document are there to provide guidance and further information on the Council's expectations for this area. As time passes, should market conditions and requirements change, it may be necessary to update this masterplan.

The masterplan will be a material planning consideration in the determination of all planning applications relative to the Knott Mill area.

**5.7.7** It is, however, essential that the early phases of development demonstrate delivery of the quality of buildings and spaces, together with the essential infrastructure, that will act as a catalyst and provide a benchmark for future phases of positive change.

**5.7.8** The illustrative masterplan identifies the opportunity to deliver height and density including a significant amount of residential accommodation along the River Medlock's edge and around the key new public space at the heart of Knott Mill. The delivery of these significant new buildings must be combined with the delivery of a high quality public realm in this area, enhanced riverside access and, ideally, the new River Medlock crossing.

**5.7.9** This key phase of development falls into three separate ownerships, with the public realm involving works to the adopted highway and works adjacent to and over the waterway. Any planning application relating to all or part of this phase of development will need to demonstrate how the total and timely development of the public realm will be delivered. This can only happen on the basis of collaboration between each of the parties involved and development agreements being put in place setting out each party's contribution to the public realm works. In terms of funding and developer contributions, it will need to be agreed by the landowners that developments coming forward will be required to contribute on a fair and proportionate basis to the total costs of delivering public realm and other strategic priorities associated with this illustrative masterplan. The exact mechanism for this is to be set out and agreed with the City Council.

**5.7.10** As development proposals move forward and further detailed design work is undertaken, discussions on the potential Section 106 obligations will take place, in accordance with Manchester City Council's Development Plan policy requirement, to ensure the best outcomes for the area.

**5.7.11** In addition to public realm enhancements which have been identified as a key priority for this area, planning obligations may include the provision

of social and community infrastructure, affordable housing, employment initiatives, transport improvements, public realm enhancement and related benefits.

**5.7.12** In terms of specific issues that may need to be addressed, the following provides an initial list:

- Improving the waterside access to, around and across the River Medlock.
- Improving east-west linkages.
- Improving connections into Great Jackson Street and to Hulme Park beyond, to Manchester city centre, to Castlefield and to First Street.
- Relocating Utilities affected by development sites where necessary and appropriate and installing new Utilities in order to provide future proof and sustainable development.
- Place-making including the delivery of public open space. Improvement of the streetscape, particularly around and below the Whitworth Street viaduct. Improving crossing facilities and traffic calming.
- Wayfinding and signage schemes.
- Healthy living environments including access to leisure, green and blue infrastructure, promotion of walking and cycling, focus on managing and enhancing air quality.
- A strong focus on providing broad appeal high quality market and affordable housing across the rental and sales market.
- Local employment initiatives
- Measures that promote safety and security in addition to a well-designed environment with natural surveillance, such as lighting.
- Retain buildings of character and make the most of natural assets.

**5.7.13** In addition to this, there is a need for each development phase to consider and make allowances and provisions for delivering the social and community infrastructure necessary to support the new residents and ensure a sustainable neighbourhood is created, and in so doing add to the city centre's overall offer. The required level of provision within each scheme will be assessed by the City Council as part of the planning application process.











Strategic Contribution of the Knott Mill Masterplan

Manchester’s Growth Agenda

Context

Greater Manchester has set out plans to provide for 200,000 jobs and forecasts GVA growth of 2.5% year on year (an uplift of £5bn above baseline conditions by 2035), based on population growth of 294,800 and the provision of over 227,000 new homes by 2035<sup>1</sup>. The provision of over 10,000 new homes per annum will require new developments on brownfield land in urban locations, particularly in well-connected urban locations, such as Manchester city centre. Significant locations for development include the city centre, The Quays, Manchester Airport and principal town centres.

Manchester city centre currently accommodates around 10% of all jobs in Greater Manchester and 1,250,000m<sup>2</sup> of new office space is planned to accommodate forecast growth, alongside some 40,000 new dwellings, including in fringe areas such as Ancoats. The significant intensification of development in and around the city centre has resulted in an increased employment base, with current levels of 140,000 jobs in the city centre and a further 60,000 jobs in the nearby Corridor Manchester.

The level of economic growth planned for the conurbation and city centre is central to the ambitions for the Northern Powerhouse and the emerging Industrial Strategy. This scale of ambition is linked to planned infrastructure investment, including the refurbished Oxford Road Station, the new HS2 station at Piccadilly and emerging plans to transform Deansgate Station and its environs.

Employment Growth

The ability of the city centre to accommodate further employment growth is linked to factors such as the size and quality of the workforce, transport connectivity and modern and affordable office accommodation for the various sectors and types of business attracted to a city centre location.

Increasing the capacity of the city centre to accommodate growth, primarily through a process of densification and modernisation, set within agreed regeneration frameworks, is an important part of capitalising on regeneration and transport investment. The need to continue to develop a pipeline of priorities to meet demand becomes more important and areas such as First Street, the Civic Quarter and Spinningfields develop out in full.

Manchester has a remarkably diverse economy and a number of sectors will provide new opportunities for further investment and growth, characterised by a young, high skilled workforce. Working Futures has provided an overview of how the Manchester workforce will change as new trends impact on occupational groups.

The Working Futures research forecasts an increase in higher level and technical skills based on:

- Financial and Business Services: a key driver of economic and employment growth in Manchester and projected to continue to grow in the coming decade. Over 40,000 jobs in the city centre are in these sectors.
- Creative and Digital: with employment growing rapidly.
- Telecommunications, Media and Technology: with future growth in Manchester’s TMT sector forecast to be strong with employment increasing by 1.1% per annum and output by 3.8% per annum, making it the fourth fastest growing segment of the city’s economy.
- Science and Technology: where Manchester is one of the UK’s leading centres for science and technology research; world leading expertise in a number of fields and an exceptionally large doctoral and post-doctoral research community now developing technologies which will shape a wide range of global industries.

The importance and scale of growth anticipated in these key sectors has been set out in the Business Sector Deep Dive reports undertaken by New Economy, commissioned by the Combined Authority to support the development of the Greater Manchester Spatial Framework.

Manchester Employment Growth: Key Sectors				
	Businesses	Employment	Jobs/Company	Jobs Growth 2015-2035
Creative	1,350	9,000	6.6	4,500
Digital	1,300	14,000	10.8	7,000
Financial Services	700	23,000	32.8	2,300
Professional services	3,700	39,500	10.7	15,800
Business services	1,350	23,000	17.0	9,200
Employment Services	400	16,500 <sup>1</sup>	41	6,600
<sup>1</sup> Includes personnel based off-site				

<sup>1</sup> Forecasts in the GMSF are based on an Accelerated Growth Scenario.

Many of the companies in the key growth sectors are micro and small companies, often with a highly skilled labour force. These entrepreneurial companies need relatively small floorplates and are clustered in areas such as Knott Mill, the Northern Quarter, Ancoats and Piccadilly Basin. These districts add to the city centre offer, particularly targeting companies unable or unwilling to pay the higher rents for Grade A space.

Other Deep Dive reports highlighted the expected growth in leisure, the visitor economy and tourism, with the cultural assets and live music venues of the city centre attracting national and international visitors. New Investment in cultural venues including The Whitworth, Art Gallery, HOME and Manchester Museum have increased visitor numbers, while the opening of Factory in 2020 is expected to further increase the cultural and visitor economy in the city centre.

Again, many of the small leisure, cultural and entertainment businesses require city centre business premises at inexpensive rents.

One of the major economic challenges facing the conurbation is providing the skilled workforce to support economic growth, and this is exacerbated by the ageing of the Greater Manchester and UK workforce and population. Greater Manchester, in effect, needs to attract and retain additional workers to support growth and this is placing additional pressure on the housing market to provide new homes for a growing workforce.

This demand for additional skilled workers to support economic growth is greatest in the city centre, reflecting the skilled workforce needed for high growth sectors and the large education and health workforce in Corridor Manchester. There is a pressing need for new high density housing to provide the homes to support both the city centre workforce and those needing to travel to other growth centres, such as Airport City and Salford Quays, taking advantage of improved public transport links.

The Residential Growth Prospectus was developed in response to very low levels of housing development over many years, a result of the 2008-2010 recession. The subsequent slow down resulted in a significant level of pent up demand for new housing and a disparity between supply and demand for quality accommodation, which is only now beginning to be addressed.

Given the scale of the shortfall in new development and the forecast employment and population growth, new development needs to continue if the City Council is to provide the high quality and diverse residential offer which is crucial to attracting and retaining the expanding workforce and in particular to retaining talented and skilled workers to support the city centre economy.

Housing Strategy for Manchester 2016 – 2021 sets out the importance of housing to economic growth where it states on the first page of the document that:

*“to guarantee the continued economic growth of the city for the benefit of all of its residents, it is essential that we continue to attract and retain working households. We must provide more housing in places that are best-connected to future employment opportunities.”*

The need to provide new business space to support economic growth, accompanied by new housing for a growing and higher skilled workforce underpins the Council’s city centre strategy of balancing higher density with distinct, attractive and different neighbourhoods to meet market demand. The scale and long term nature of employment and household growth requires the City Council to have both advanced plans, often with seven to ten year development periods, and a pipeline of new spatial and regeneration frameworks to maintain the momentum needed to match long term demand.

Given the new trend for tower developments, between 30 and 60 storey mixed use blocks, with commercial and hotels on lower floors, there is a need to maintain and strengthen those parts of the city centre with a strong urban fabric and identity. One of the city centre’s strengths is the distinctiveness of neighbourhoods and districts able to attract a variety of business, from the corporate HQ to the micro and independent businesses.

The table below sets out the main developments through which the city centre will meet the demand for new business space and housing from a fast growing economy. The list includes well established districts and a number which are still at the planning stage.

	Key City Centre Districts
Spinningfields	New and very successful city centre corporate business district – 10 major office blocks and two under development, now accommodating circa 20,000 people. Tower blocks a new feature.
St Johns	Former Granada TV studios, now anchored by Factory (new £110m cultural venue). The development will see up to 2,500 homes, up to 600,000 ft <sup>2</sup> of workspace, mostly aimed at creative industries, and 400,000 ft <sup>2</sup> of leisure space. Tower blocks for some residential
First Street	New city centre mixed used development, with HOME (new gallery and cinema funded by ACE), residential blocks including student accommodation, hotels, and new offices with large floorplates. Next to the Civic Quarter and Oxford Road.
NOMA	Next to the now refurbished Victoria Station and MEN Arena, with Cooperative Group leading with the redevelopment of their corporate HQ – now being taken forward by foreign investment
Civic Quarter	Developed around Town Hall Extension refurbishment, the Town Hall Refurbishment and St Peters Square. New offices are setting record rents, and a new tower block is planned at St Michaels.

Oxford Station	Circle Square under development to deliver 1,200 homes, 1.2m ft <sup>2</sup> of commercial space. Tower blocks feature. Oxford Road Station redevelopment, with new housing and leisure space.
Ancoats/New Islington	City centre fringe, former low demand mixed use area being turned into apartment market with some commercial development, now backed by Manchester Life (with Abu Dhabi Investment)
Piccadilly and Mayfield	Major investment in commercial space linked to new HS2 station. Mayfield proposals agreed to provide 1,700 homes, 1.5m sq ft <sup>2</sup> of office space, a 200-bedroom hotel and retail and leisure facilities.
Knott Mill	Established city centre mixed use area, with the potential for modernisation of the commercial space and additional housing, build on current streetscape, and market demand from digital, creative and professional service companies
North Campus	Close to Piccadilly and Oxford Road. Likely to include significant new residential development, and re-use of some buildings for creative and digital companies.

Knott Mill offers a very distinctive small business and residential district, alongside the Northern Quarter and parts of Piccadilly Basin and Ancoats, which offer business accommodation for micro and small companies in creative, digital and professional services.

### Knott Mill

#### Employment and Business Base

The Knott Mill area contains a mixture of uses, with a diversity of building styles and differing scales. It is a low to medium rise area, with potential for denser and taller development, given the scale of new development underway in adjoining areas.

There are circa 118 companies in the Knott Mill area employing circa 600 people. There are a wide variety of businesses including 12 architectural practices, over 20 media and marketing companies, over 10 recruitment agencies and upwards of 20 specialist consultancy companies. In addition, there are some 400 residential units in the area.

The number and diversity of the business base highlights the important role which Knott Mill plays in providing affordable business space for many micro and small business in the creative, digital and professional services. Many of these business are a crucial part of the supply chains for larger businesses and important institutions in the city economy.

#### Additional Commercial Development

The Masterplan will contribute to the economic and housing ambitions of the City Council and Greater Manchester Combined Authority and new development will be commercially led.

The **proposed development will** provide 226,699 sq ft of commercial space. This will be complemented by the modernisation of some existing commercial space, some 49,364 ft<sup>2</sup> of existing commercial space will be displaced, resulting in a net additional figure of 177,334 ft<sup>2</sup> (GIA). Under this option, the net additional commercial space will accommodate some 920 additional jobs, increasing employment in Knott Mill significantly. The new development will maintain and strengthen Knott Mill's role as an important and distinctive area for micro and small businesses in the creative, digital and professional services sections, complementing the new business space being developed in areas such as First Street and St Johns.

The new jobs accommodated will provide a broad range of opportunities, including a considerable number of graduate level positions in technical occupations, reflecting the current sectoral profile and employment forecasts for the city. There will be a significant number of entry level jobs as well as part time opportunities, both of which will contribute to local employment and the inclusive growth agenda.

Employment Capacity of Additional Business Space (NIA)						
	Amount NIA	FTE	Jobs	Level 4/5	Level 3	Level 1/2
Office	76,575ft <sup>2</sup>	593	711	363	174	174
Retail / leisure	25,965ft <sup>2</sup>	138	184	42	47	120
Hotel	21,594ft <sup>2</sup>	19	25			
Total	124,135ft <sup>2</sup>	750	920	405	221	294

Alongside the additional jobs accommodated in the net additional commercial space, the displacement and then replacement of some existing commercial space will result in more modern and efficient premises to accommodate some of the existing employment.

#### Additional Residential Development

The final make-up of the new residential development will reflect market demand. This is likely to focus on one and two bedroom apartments (see following table), with a small number of three bedroom apartments. This balance reflects the residential demand set out by the City Council.

Potential Additional Residential Development				
	1 bedroom	2 bedroom	3 bedroom	Total
Commercial-focussed option	64	83	8	154

#### Economic Impact

The new commercial development will make an important contribution to the accommodation of economic growth in the city centre. An additional £45.8m GVA will be generated by companies in the new business space, whilst the residential-focussed option would also add to GVA levels in the city.

Economic Impact		
	Employment FTE	GVA
Office	593	£38.5m
Retail / leisure / hotel	157	£7.3m
Total	750	£45.8m

The new employment space and jobs accommodated will generate considerable additional taxation income for both Government and the City Council.

Taxation Income			
	Business Rates	Income Tax	National Insurance
Office	£1.0m	£3.0m	£2.0m
Retail / leisure / hotel	£0.7m*	£0.4m	£0.4m
Annual Total	£1.7m	£3.4m	£2.4m
10 Year Total	£16.6m	£34.1m	£24.2m

\*Does not include hotel business rates, which will be dependent on turnover levels

The development of new commercial business space and residential units will require a large construction workforce over a five year period. This scale of activity will also support a considerable number of apprenticeship places. There will also be ne47w opportunities for the city's construction supply chain companies.

Construction Employment						
	Year 1	Year 2	Year 3	Year 4	Year 5	Total
Expenditure	£12.9m	£22.6m	£22.6m	£25.8m	£6.5m	£90.3m
Workforce	143	250	250	286	72	n/a
Apprenticeships	4	7	7	8	2	28

#### Conclusion

The outcomes of the Knott Mill Masterplan will contribute to the economic and housing ambitions of the City Council and Greater Manchester Combined Authority, as well as the wider aims sets out in Our Manchester. The Masterplan will modernise and strengthen a district which provide less expensive office and retail/leisure space for small technical services companies and independent traders, two groups which are in danger of being crowded out of the city centre to make way for new corporate development and investment.

The new jobs accommodated, over 920 in total, will provide a broad range of opportunities, including a considerable number of graduate level positions in technical occupations, and a significant number of entry level jobs as well as part time opportunities, contributing to the local employment and the inclusive growth agenda of the City Council. This new employment, along with the current workforce, will generate significant taxation income over the next ten years for the Treasury and City Council

The new residential development, the majority of which will be 1 and 2 bedroom, will help accommodate the skilled workforce needed to support economic growth. The combination of new housing and commercial development will renew and modernise a distinctive and authentic mixed use district at the heart of the city centre.











7.0 Acknowledgements

**7.1** This masterplan has been prepared by the Knott Mill Association, representing the major landowners within the Knott Mill area;

- AJ Bell Trustees Limited
- Aviva Investors Ground Rent GP
- Bolted Horse Limited
- Cabot Trustees Limited
- Mark Canning
- Canning O’Neill Group SSIP
- Citychance Limited
- DWF LLP
- Rachel Haugh
- Gaythorn Properties Limited
- Andrew Ince
- In-fill Design Limited
- The trustees of James Dickinson SSIP
- Jordan Street Limited
- Conrad O’Neill
- Open Span Limited
- Peter Rogers
- Ian Simpson
- Robert Taylor
- Unitex Trading Limited
- Whitehall Trustees Limited
- Whitehead Trustees Limited
- Zerum Jordan Street Limited

with assistance from Eksogen, How Planning and SimpsonHaugh.